

Re: Greve - Gottschalk - East Lake Sammamish Trail Segment 2B Comments to the City Council

Lindsey Ozbolt

Mon 2/6/2017 12:51 PM

To: b.greve@comcast.net <b.greve@comcast.net>;

Dear Mr. Greve,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development

425.295.0527

From: b.greve@comcast.net <b.greve@comcast.net>

Sent: Saturday, January 28, 2017 1:38 PM

To: City Council

Cc: Jeffrey Thomas; Lyman Howard; Jessi Bon; David Pyle; Kim Adams Pratt; Lindsey Ozbolt; Christie Malchow; Tom Hornish; Ramiro Valderrama-Aramayo; Gus Gottschalk

Subject: Greve - Gottschalk - East Lake Sammamish Trail Segment 2B Comments to the City Council

Good morning City Council Members -

In a dialog (shown below) with council member Christie Malchow earlier this month seeking guidance on how best to ensure the city council had the opportunity to understand "each individual homeowner's impacts and concerns", and to help the council when the council meeting or "study session" takes place in reference to the East Lake Sammamish Trail - Segment 2B, we were told to forward comments to the city council as well as Ms. Ozbolt.

Our neighbors (William (Gus) and Debra Gottschalk) and us (William and Kathryn Greve) worked jointly with our attorney to develop our comments as we share a private drive leading into our properties. Our properties are part the Waterside Home Owners Association.

Please find our joint comments and associated exhibits attached in pdf form.

Note that together and with Gus' 35 years of commercial construction experience as President of

Lydig Construction, we have identified clear and specific alternatives to each of our concerns relating to the 60% design plan. Our proposals do NOT impact the design intent of the trail, but instead creates a far safer, more cost effective, and rational design. In fact, our proposal works to acknowledge and adhere to two specific design objectives outlined in King County's communications which are being unmistakably averted with the current 60% plan. The two objectives referenced include:

- (1) "[m]inimizing costs where possible without impacting trail standards," and
- (2) "[m]inimizing impacts to adjacent homeowners."

We view many of the design elements in the 60% plan as unnecessarily impactful; especially in light of the alternatives. They also significantly elevate the risk to trail users as it relates to the sight lines associated with the trail crossing both exiting and entering our properties. It's for these reasons that we worked so diligently to not just object to the impactful elements of the plan but to instead use common sense and best practice design considerations to create and share clear and specific alternatives that satisfy each concern and work to what we feel can be a mutually agreeable solution.

Ultimately we want to see this project succeed and become the wonderful shared resource that it can be, but not at the cost or with the unnecessary impact designed into the current 60% plan.

Please inquire should you have any questions, need any additional information, or best case if you would like to set time for us to discuss, demonstrate, and/or explain not just our concerns, but our rationale.

With Best Regards and Intentions,

William (Bill) and Kathryn (Katy) Greve
William (Gus) and Debra (Debbie) Gottschalk

From: "Christie Malchow" <CMalchow@sammamish.us>
To: "Jeffrey Thomas" <JThomas@sammamish.us>, "b greve" <b.greve@comcast.net>, "City Council" <citycouncil@sammamish.us>
Cc: "Lyman Howard" <lhoward@sammamish.us>, "Jessi Bon" <JBon@sammamish.us>, "David Pyle" <DPyle@sammamish.us>, "Kim Adams Pratt" <kim@kenyondisend.com>, "Lindsey Ozbolt" <LOzbolt@sammamish.us>
Sent: Monday, January 16, 2017 9:40:46 PM
Subject: RE: Seeking Guidance - East Lake Sammamish Trail Segment 2B

Thank you, Jeff, for clarifying.

Christie Malchow
Sammamish City Council
cmalchow@sammamish.us
(425-301-6667 | www.Sammamish.us
801 228th Ave SE | Sammamish, WA 98075



From: Jeffrey Thomas
Sent: Monday, January 16, 2017 6:46 PM
To: Christie Malchow <CMalchow@sammamish.us>; b.greve@comcast.net; City Council <citycouncil@sammamish.us>
Cc: Lyman Howard <lhoward@sammamish.us>; Jessi Bon <JBon@sammamish.us>; David Pyle <DPyle@sammamish.us>; Kim Adams Pratt <kim@kenyondisend.com>; Lindsey Ozbolt <LOzbolt@sammamish.us>
Subject: Re: Seeking Guidance - East Lake Sammamish Trail Segment 2B

Hi Christie,

One clarification and one correction from your email earlier today to Mr. & Mrs. Greve:

1. Clarification - City staff is reviewing and compiling public comments as they are submitted through next week. The public comments will help City staff complete its comprehensive first review of the shoreline permit application. In addition to requesting the County to respond to the public comments, the City will also determine requested revisions and send to the County concurrently.

2. Correction - As currently set up, the shoreline permit application is being processed as a Type II permit - the Community Development Director issued the decision on behalf of the City. As we learned from the State Shorelines Hearings Board with south segment 2a, the Hearing Examiner does not have jurisdiction to hold an administrative appeal hearing on a shoreline permit decision issued by the Director. Therefore the appeal of a shoreline permit decision will go directly to the State Shorelines Hearings Board.

Thanks, Jeff

From: Christie Malchow
Sent: Monday, January 16, 2017 1:18 PM
To: b.greve@comcast.net; City Council
Cc: Lyman Howard; Jessi Bon; Jeffrey Thomas
Subject: RE: Seeking Guidance - East Lake Sammamish Trail Segment 2B

Mr. & Mrs. Greve,

I've cc'd a few staff members here to elaborate or correct any misinformation/omitted information in my response below (in red). My answers below are based on the best of my knowledge and are process based to help you on the questions you've asked below.

Christie Malchow
Sammamish City Council
cmalchow@sammamish.us
(425-301-6667 | www.Sammamish.us)



From: b.greve@comcast.net [<mailto:b.greve@comcast.net>]
Sent: Monday, January 16, 2017 9:55 AM
To: City Council <citycouncil@sammamish.us>
Subject: Seeking Guidance - East Lake Sammamish Trail Segment 2B

Good morning

This e-mail is specifically created to ask for clarification and direction about the city's role and the processes in reference to the on-going and vitally important issues involving the East Lake Sammamish Trail - Segment 2B project.

We (Bill and Katy Greve) residing at 2417 E Lake Sammamish PL SE respectfully request information and answers to each of the following questions outlined below.

- Correspondence coming from both the City of Sammamish and King County provided direction for property owners to submit comments to the staff project planner (Lindsey Ozbolt). Upon doing that an automated response was received stating "Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response". This response seems to elude to the fact that the City of Sammamish is merely a "pass through" for the process by simply collecting the comments and sending them off to King County without working to understand, building a case, offering opinion, or advocating for its citizens..
 - Will the City Council actively review the comments provided, seek to understand them in detail, and ultimately advocate for the citizens of Sammamish? **We certainly can read them, but they are not given to Council specifically. You can email your comments to the Council at citycouncil@sammamish.us, this will help us to better understand each individual homeowner's impacts and concerns. This will then help us when we have the County in for a Council meeting or study session.**
 - Will the City Council actively participate and help to mediate discussions between King County and the citizens of Sammamish to resolve issues to citizen satisfaction? **We are certainly advocating for a study session or the like where King County is present, to answer our questions & citizens alike, so yes, we will be actively participating in discussions between the County, citizens, and City staff processing the applications.**
 - Who specifically makes the decision to issue both the shoreline substantial development permit and the clearing and grading permit; and what influence does the city council have in that process? **City of Sammamish's staff. The Council does not have influence in reality there, aside from encourage legal & staff to scrutinize the application for meeting our City's codes and regulations.**
 - Does the City Council have the ability to prevent either permit (SSDP and Clearing & Grading) from being issued? **No, not to my knowledge.**
 - What specifically is the procedure to surface issues and seek adjustment to the proposed 60% plan; aside from simply submitting comments? **Submitting your comments is the primary means, and certainly engaging Council in those comments (via public comment or**

simply by emailing them to us). The more we know, the better we can advocate for alterations to the design plan that allows the trail to proceed, but also takes into account affected trail-side owners' issues.

- Will the City Council actively be involved in and support citizens in discussions involving proposed adjustments to the 60% plan? I think the entire Council has an interest in the trail. I certainly do. As far as alterations to the plan, staff will ultimately make those decisions. The Council is certainly going to weigh in on the trail, and as of last Tuesday has asked for a joint meeting that would have King County officials in for a meeting that would likely be a study session. There was an urgency on this request, & I know our City Manager has already reached out to the County on this meeting, I would anticipate that meeting sooner than later.
- In the event that King County does not work to address the proposed adjustments to citizen satisfaction, what is the specific process to appeal, mediate, and mitigate the situation to ensure satisfactory results, and what role with the City play in this process? The appeal can be done if the City approves the plans (after the final submission based on 100% design plan is reached). At that point any group or individual may appeal the decision to the Hearing Examiner.

Citizens have spent literally hundreds of hours trying to understand how to be heard and how to ensure the slightest bit of comment sense and rational thought is applied to the issues being forced upon us or suggested changes. We've worked to submit comments in multiple forms and forums as directed, but no impacted party feels good about how the process has unfolded thus far. Most feel completely unsupported by the city and certainly stonewalled by the county.

I understand your frustrations. My responses above are intended to shed a bit of light on process for you. However, if you feel you have more questions, please don't hesitate to email Council or call me. My cell phone number is listed below in my email signature.

Satisfactory and complete answers to the above questions will at minimum help to ensure we know what to do and how to do it.

Please advise.

Sincerely,

Bill Greve



LAW OFFICE OF
SAMUEL A. RODABOUGH PLLC

SAMUEL A. RODABOUGH
ATTORNEY AT LAW
11820 NORTHUP WAY, STE. E200
BELLEVUE, WA 98004
(425) 440-2593
(425) 284-3051 (FAX)

January 27, 2017

Via Email & U.S. Mail

City of Sammamish
Department of Community Development
Attn: Lindsey Ozbolt, Associate Planner
801 228th Ave. SE
Sammamish WA, 98075
lozbolt@sammamish.us

King County
Department of Natural Resources and Parks
Attn: Gina Auld, Capital Project Manager IV
201 S. Jackson St., Ste. 700
Seattle, WA 98104-3855
gina.auld@kingcounty.gov

**Re: Shoreline Substantial Development Permit 2016-00415
East Lake Sammamish Trail, South Sammamish B Segment**

Dear Ms. Ozbolt and Ms. Auld:

This Firm represents William & Debra Gottschalk (collectively "Gottschalk") and William & Kathryn Greve (collectively "Greve"), the owners of residential properties located within the City of Sammamish ("City"). My clients' properties will be adversely affected by the proposed modifications to the East Lake Sammamish Trail, South Sammamish B Segment ("Trail") that have been proposed by King County ("County") in the above shoreline substantial development permit ("SSDP"). My clients are in receipt of the City's Notice of Application for the above SSDP and they have reviewed the 60% design plans for the Trail, dated on or about September 2016 ("Preliminary Plans"). Please accept the following as (1) a response on behalf of my clients to the SSDP application, including the Preliminary Plans, and (2) a request for my clients to be included as parties of record for this SSDP and to receive future notifications and status updates regarding the SSDP application.

A. The Properties

Gottschalk owns and resides in the residence located at 2419 E. Lk. Sammamish Pl. SE, Sammamish, WA 98075, also known as King County Tax Parcel No. 0724069055 ("Gottschalk Property"). Greve owns and resides in the adjoining residence located at 2417 E. Lk. Sammamish Pl. SE, Sammamish, WA 98075, also known as King County Tax Parcel No. 0724069059 ("Greve Property"). The Greve Property is located immediately north of the Gottschalk Property. As with many waterfront properties in this area, the Gottschalk Property and the Greve Property are physically constrained by Lake Sammamish to the west and the Trail to the east. Although these properties enjoy significant waterfront amenities, they are also characterized by significant access constraints and privacy concerns stemming from their proximity to the Trail.

By way of background, and for purposes of this letter, with the limited time available for public comment, my clients have been unable to undertake a comprehensive review of the titles to their respective properties to determine the origin of the County's right-of-way for the Trail.

However, per maps available through the County's Department of Natural Resources and Parks, it appears that the origin of the right-of-way in this section of the Trail is the "Tibbetts Deed."¹

The map does not explain if the County believes it owns a fee simple interest in this section of the Trail, or a mere easement. In this limited time available for public comment, however, my clients have been unable to verify if the property interest conveyed by the Tibbetts Deed has previously been adjudicated by any state or federal court. Nonetheless, until demonstrated otherwise, similar to other sections of the Trail, my clients' necessarily take the position that the County's interest constitutes an easement and that my clients own the underlying fee simple interest.

B. Deficiencies in Preliminary Plans

As indicated, my clients have reviewed the Preliminary Plans for the Trail. In this regard, it is worth noting that Mr. Gottschalk has over 35 years of complex construction experience. He is currently the President of Lydig Construction, Inc., a regional commercial construction company whose project portfolios include federal, state, and local government buildings (*e.g.*, secondary and higher education buildings, courthouses, administration buildings, correction centers, civic halls, etc.) and private commercial buildings (*e.g.*, offices, hospitals, hotels, casinos, etc.). In short, Mr. Gottschalk is well-versed and highly qualified in reviewing construction drawings. Accordingly, my clients offer the following comments regarding the Preliminary Plans:

1. Unnecessary Waterward Realignment of Trail Centerline

Per the Preliminary Plans, it appears that the County is unnecessarily realigning the centerline of the Trail waterward (*i.e.*, closer to my clients' residences).² Notably, the County has previously published the criteria that it employs to determine if the existing centerline of the Trail should be realigned, which include the following: (1) "[m]inimizing costs where possible without impacting trail standards," and (2) "[m]inimizing impacts to adjacent homeowners."³ As explained in greater detail below, it does not appear that the County's proposed realignment complies with either of these criteria.

¹ See East Lake Sammamish Trail Railroad Right of Way Historical Acquisitions, King County Department of Natural Resources and Parks, Parks Division (July 29, 2014), at pg. 15.

² Compare Preliminary Plans, Existing Conditions Plan, at pg. EX6 (attached hereto as Exhibit 2) with Plan and Profile, at pg. AL10 (attached hereto as Exhibit 1).

³ East Lake Sammamish Trail Project, King County Parks (Spring 2014), at pg. 5.

Specifically, the proposed realignment occurs between stations 327+31.99 and 326+71.62.⁴ The realignment results in the following significant, adverse impacts, among others:

- **Reduced Utility of Shared and Separate Driveways** – The realignment shortens the approach to the shared portion of my clients' driveway and severely limits vehicle maneuverability and ingress and egress from the easternmost portions of their separate driveways. In particular, the turning radius of their driveways are significantly compromised and may require the owners to trespass onto each other's property for future, rudimentary driveway navigation.
- **Reduced Safety/Visibility** – The proposed Trail realignment creates an increased safety hazard for both vehicles and Trail users at this crossing. Specifically, the rather abrupt realignment near the north property line of the Greve Property appears to reduce sight distance for vehicles exiting the shared portion of my clients' driveway, which decreases safety for both my clients and Trail users.
- **Proximity, Loss of Privacy and Safety** – The proposed Trail realignment will undoubtedly negatively affect the values of my clients' residences, both of which are multi-million dollar residences. The proposed Trail realignment and accompanying widening will require the loss of most, if not all, of the existing privacy screening for these residences, including mature arborvitae hedges. In short, Trail users will not only be much closer to these residences, but will be staring through windows into their homes. Additionally, the increased proximity of the Trail to my clients' residences may encourage Trail users to engage in unauthorized use of the highly visible boat launch located on the Greve Property.

2. Inadequate Drainage Infrastructure

The existing elevated Trail corridor currently acts as a berm that collects surface water behind it during extreme weather conditions. This problem is exacerbated by excess hydraulic water pressure from Jurisdictional Ditch #11B and runoff from nearby impervious surfaces, including the existing semi-permeable gravel Trail.⁵ Although the Preliminary Plans depict the existence of four, 6-inch culverts located near the north end of Jurisdictional Ditch #11B,⁶ these culverts do not currently provide an outlet for the ponding water. Instead, because the ponding water currently has no outlet, it builds hydraulic pressure that adversely affects the foundations and sewer systems of both the Gottschalk and Greve residences. This hydraulic pressure has led to water infiltration through the foundations and into their respective residences.

⁴ See Preliminary Plans, Plan and Profile, at pg. AL10 (attached hereto as Exhibit 1).

⁵ See Preliminary Plans, Existing Conditions Plan, at pg. EX6 (attached hereto as Exhibit 2) with Plan and Profile, pg. AL10 (attached hereto as Exhibit 3).

⁶ See Preliminary Plans, Existing Conditions Plan, at pg. EX6 (attached hereto as Exhibit 2).

The following photos depicts the water that ponds behind the Trail corridor in front of my clients' residences and the damage to these residences as a result of this ponding and associated hydraulic pressure:



*Note – The above photo was taken at approximately 3:00 p.m. on January 18, 2017. The ditch collects and retains water during extreme weather conditions. The ditch was water free 18 hours prior to the time that this photo was taken. As explained in greater detail herein, adopting my clients' recommended drainage improvements, will resolve the existing drainage issues and better protect any Trail improvements from unnecessary erosion and damage.



*Note – The above photo depicts the source of water forced up through the foundation of the residence as a result of hydraulic pressure.



*Note – The above photo depicts the pathway by which water, forced up through the foundation from hydraulic pressure, runs along the interior walls of the residence.

The proposed drainage improvements in the Preliminary Plans do not appear to adequately address these drainage concerns. In particular, changing the Trail from a semi-permeable gravel surface to an impervious paved surface, while simultaneously widening the Trail, will increase surface water runoff. Moreover, the Preliminary Plans do not depict any underdrain in the vicinity of my clients' properties that will allow for surface water collecting on the east side of the Trail to drain to the west side and ultimately be discharged into the Lake. In other words, it is likely that the existing ponding conditions will continue unless and until the Preliminary Plans are revised with respect to drainage.

3. Design

My clients, including Mr. Gottschalk with his extensive design and construction experience, believe that the Proposed Plans depict a Trail with poor design and a general lack of consideration to architectural exterior design. Specifically, the Preliminary Plans include a masonry retaining wall with a coated chain link for only a portion of affected property, and leaving the remainder with no protection at all. This total lack of architectural perspective by the County fails to follow any reasonable architectural standards for the proposed improvements. The County should have designed something more consistent with the existing improvements that takes into consideration that the two residents share one common entrance and the architectural barrier should be consistent along the affected property.

B. Proposed Resolutions for Deficiencies in Preliminary Plans

My clients believe that there are simple and cost-effective design solutions that would largely alleviate the above concerns that are both (1) consistent with the County's design objectives for the Trail, and (2) avoid negative impacts to adjacent property owners. These solutions are as follows:

1. Shift Proposed Realignment of Trail Centerline to the South

My clients propose that the abrupt transition for the Trail centerline realignment currently depicted as occurring between stations 327+31.99 and 326+71.62 be shifted to the south between stations 324+50 and 324+00.⁷ It does not appear that shifting the transition to that location would impact any adjacent properties, as that location does not involve constraints that are similar to those in the immediate vicinity of my clients' property. For example, unlike the County's proposed location, my clients' proposed location is not in the vicinity of a Trail crossing, such as a driveway. Moreover, my client's proposed location for the transition would alleviate concerns regarding impaired sight lines at my clients' Trail crossing, as the Trail alignment could be straightened in the absence of the proposed transition. My clients' proposal would also accommodate the following:

- **Retaining Wall #10** – My clients' preferred alignment would allow for Retaining Wall #10 to be moved east, closer to the alignment of the Trail, which could then be reengineered to be either a smaller retaining wall, or be eliminated altogether as a result of existing elevations. This common sense change would result in considerable savings to taxpayers.⁸
- **Clearing and Grubbing Limits** – My clients also propose that the clearing and grubbing lines be modified to correspond to my clients' preferred Trail realignment. My clients' proposed modifications are depicted on the attached Exhibit 3. Further, the clearing limits should be adjusted to follow the course of the Trail in order to prevent and/or limit, any adverse impacts to my clients' existing stamped concrete driveway, irrigation, drainage, and landscape lighting.
- **Drainage Revisions** – My clients also request that certain changes be made to the Preliminary Design with respect to drainage, as depicted in the attached Exhibit 4. These proposed changes are summarized as follows:

⁷ See Preliminary Plans, Existing Conditions Plan, pg. EX6 (attached hereto as Exhibit 2).

⁸ See Preliminary Plans, Existing Conditions Plan, Plan and Profile, pg. AL10 (attached hereto as Exhibit 3).

(1) Continue the underdrain depicted for installation south of station 326+00 on the east side of the Trail through to station 327+31.99. Tie the underdrain to Catch Basin #9 located at station 327+34.

(2) To address the additional ponding that will be expected from increasing the impervious surface from the Trail due to widening, my clients request the installation of a CMP slotted trench drain in the existing driveway, such as the product available from Contech Engineering Solutions depicted in Exhibit 6.

- **Fencing** – My clients also request that they be allowed to maintain the existing level of safety and security that exists for their properties, which will be significantly compromised by the removal of their vegetative privacy screening, existing fence, and electric gate. Maintaining the same level of security will also eliminate the potential for unauthorized use of the highly visible boat launch located on the Greve Property. My clients recommend realigning the chain link fence depicted in the Proposed Plans consistent with their preferred Trail realignment and extending said fence across both properties as depicted in Exhibit 5. Further, they request permission to install an electric rolling security gate similar to existing one serving the properties. Doing so will also maintain a reasonable resemblance of the exterior architecture of these multi-million dollar homes.

CONCLUSION

The Trail constitutes a regional asset that is beneficial to the greater public. As such, my clients do not oppose improvements to the Trail and sincerely desire that the project will be successful and completed in a timely manner. However, my clients justifiably believe that the proposed Trail improvements should consider the adverse impacts to adjoining properties (as expressly set forth in the County's own criteria), including the Gottschalk Property and Greve Property. My clients respectively request that the County give their proposed improvements serious and thoughtful consideration, as the adoption of those proposals would remedy their concerns.

Sincerely,

LAW OFFICE OF SAMUEL A. RODABOUGH PLLC



Samuel A. Rodabough

sam@rodaboughlaw.com

cc: Barbara Flemming, Senior Deputy Prosecuting Attorney

GENERAL NOTES:

1. SEE SHEET G4 FOR SYMBOL AND LINE LEGEND.
2. THE RAIL ROAD CENTERLINE (RR C/L) & R/W LINES SHOWN IS BASED ON THE RIGHT OF WAY DRAWINGS PROVIDED BY KING COUNTY, DATED 1998, UNLESS OTHERWISE NOTED.
3. OUTLINES FOR BUILDING STRUCTURES SHOWN ARE BASED ON GIS AND AERIAL PHOTOS, DATED 2012.
4. NOT ALL HEDGES AND ARBORVITAE ARE INCLUDED IN THE SURVEY.
5. UTILITY ASBUILT AND POTHOLOG RESEARCH HAVE NOT BEEN INCORPORATED.
6. MODIFICATIONS WERE DONE TO THE RIGHT OF WAY LIMITS PER SUPPLEMENTAL RECORD DOCUMENTATION AS PROVIDED BY KC.
7. PARCEL LINES WERE UPDATED PER GIS INFORMATION PROVIDED BY COUNTY, DATED 2016.

LEGEND:

FORMER RAILROAD CENTERLINE 220 RR C/L

CONSTRUCTION CENTERLINE 220+00 B-LINE

REVISIONS

NO.	DATE	BY	DESIGNED
			M. TSUN
			B. PURGANAN
			P. JOHANNESSEN
			Y. HO

DESIGNED M. TSUN
DRAWN B. PURGANAN
CHECKED P. JOHANNESSEN
APPROVED Y. HO

FILE NAME BL1521075P19103EX-01
DATE 5/5/15
DATE 1521-075 P19 T03
DATE SEPTEMBER 2016

ONE INCH AT FULL SCALE, IF NOT, SCALE ACCORDINGLY

PARAMETRIX
 ENGINEERING, PLANNING, ENVIRONMENTAL SCIENCES
 719 2ND AVENUE, SUITE 200 | SEATTLE, WA 98104
 P 206.994.3700
 WWW.PARAMETRIX.COM

PROJECT NAME
 EAST LAKE SAMMAMISH MASTER PLAN TRAIL SOUTH SAMMAMISH SEGMENT B SAMMAMISH, WA

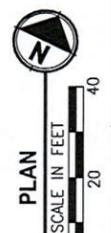
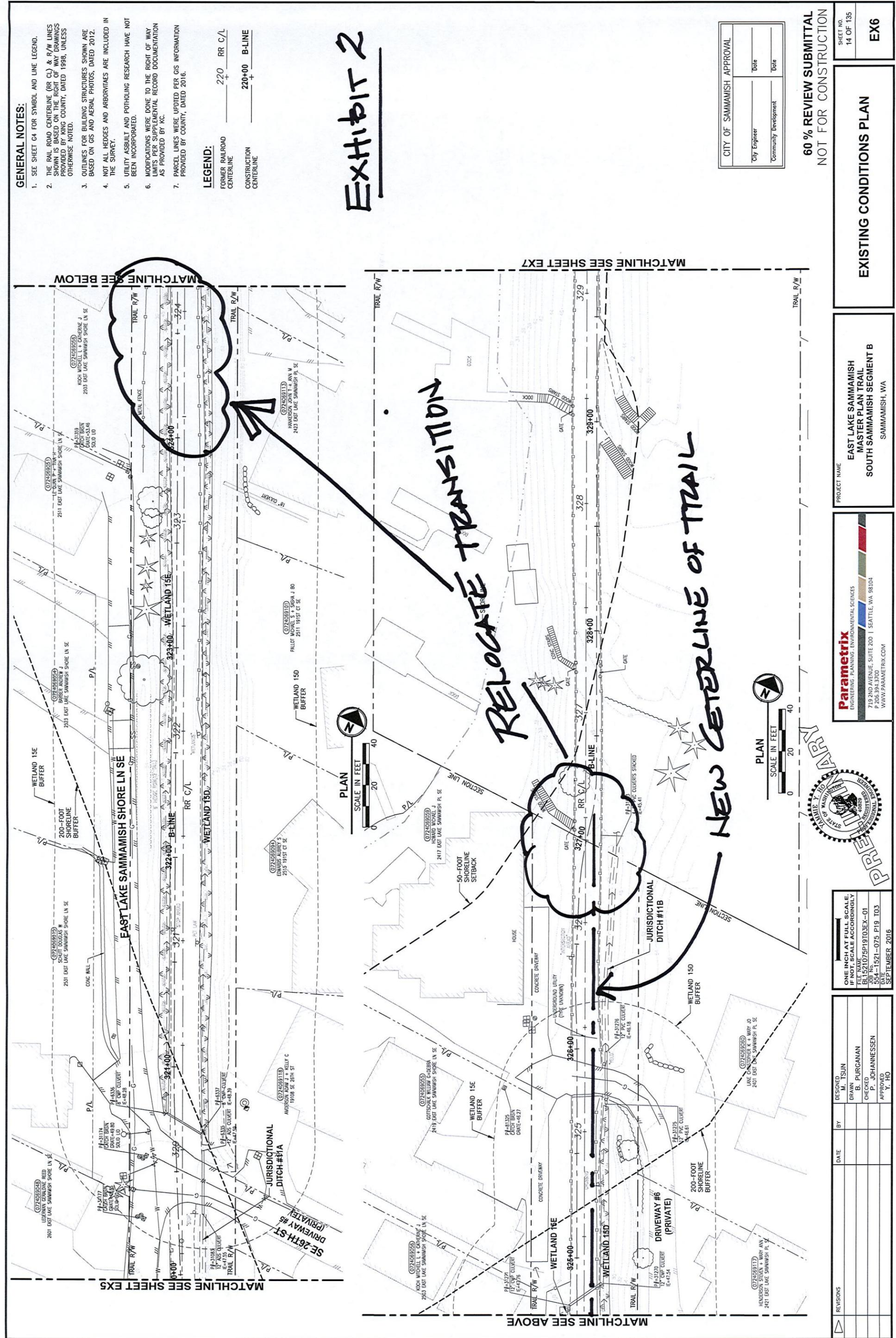
60% REVIEW SUBMITTAL NOT FOR CONSTRUCTION

EXISTING CONDITIONS PLAN

SHEET NO. 14 OF 135
EX6

CITY OF SAMMAMISH APPROVAL

City Engineer	Date
Community Development	Date



- CIVIL CONSTRUCTION NOTES:**
- RESTORE AND INSTALL STAIR, TYPE TO BE DETERMINED.
 - RECONSTRUCT PEDESTRIAN BRIDGE.
 - INSTALL PRECAST REINFC CONC SPLIT BOX CULVERT. SEE FP SHEETS FOR DETAILS.
 - INSTALL STRUCTURAL EARTH WALL. SEE WALL DETAILS ON SHEET X. SEE WALL PROFILES ON WP SHEETS.
 - INSTALL COATED CHAIN LINK FENCE TYPE 6. FOR GROUND POST INSTALLATION SEE APPENDIX B IN THE SPECIFICATIONS. SEE SHEET X FOR MOUNTING ON TOP OF WALL.
 - NEW DRIVEWAY.
 - INSTALL WOOD GUARDRAIL. SEE DETAILS ON SHEET XX.
 - INSTALL SPLIT RAIL FENCE. SEE DETAILS ON SHEET XX.
 - INSTALL GRAVITY BLOCK WALL. SEE WALL DETAILS ON SHEET WD1. SEE WALL PROFILES ON WP SHEETS.
 - DRIVEWAY REMOVAL. SEE WP SHEETS FOR PLANTING PLAN.
 - INSTALL PATTERNED CONCRETE DRIVEWAY. SEE DETAILS ON SHEET XX.
 - INSTALL TYPE 1 REST STOP. SEE DETAILS ON SHEET XX.
 - INSTALL TYPE 2 REST STOP. SEE DETAILS ON SHEET XX.
 - INSTALL SOLDER PILE WALL.
 - INSTALL GRAVITY BLOCK WALL.
 - INSTALL WING WALL. SEE FP SHEETS.
 - INSTALL EXTRUDED CURB PER WSDOT STD PLAN F-10.42-00.
 - REST STOP PULL-OUT.
 - STAIRS WILL BE ELIMINATED DURING CONSTRUCTION.
 - BRIDGES WILL BE REMOVED TO R/W LINE DURING CONSTRUCTION.
 - INSTALL ROCK WALL.

- STORMWATER CONSTRUCTION NOTES:**
- INFILTRATION TRENCH, SEE DETAIL 4 ON SHEET DD1.
 - UNDERDRAIN TRENCH, SEE DETAIL 5 ON SHEET DD1.
 - ROCK PAD, SEE DETAIL 2 ON SHEET DD1 AND WP SHEETS.
 - CONNECTION TO DRAINAGE STRUCTURE AND CLEANING EXISTING DRAINAGE STRUCTURE.
 - INSTALL UNDERDRAIN TRENCH CLEANOUT, SEE DETAIL 6 ON SHEET DD1.
 - INSTALL RECTANGULAR SLIP RESISTANT SOLID METAL COVER PER WSDOT STD DETAIL B-30.20-02.
 - CONCRETE HEADWALL, SEE DETAIL 1 ON SHEET DD1.
 - BASIC DISPERSION, SEE SHEET CS3.
 - QUARRY SPALL OUTFALL PROTECTION. SEE DETAIL 3 ON SHEET DD1.
 - ADJUST CATCH BASIN.

- LEGEND:**
- RETAINING WALL
 - SOLDER PILE WALL
 - WOOD GUARDRAIL
 - DISPERSION AREA
 - DRIVEWAY REMOVAL

CITY OF SAMMAMISH APPROVAL

City Engineer _____ Date _____
 Community Development _____ Title _____

60 % REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

SHEET NO.
42 OF 135

AL10

PLAN AND PROFILE

EAST LAKE SAMMAMISH
MASTER PLAN TRAIL
SOUTH SAMMAMISH SEGMENT B
 SAMMAMISH, WA

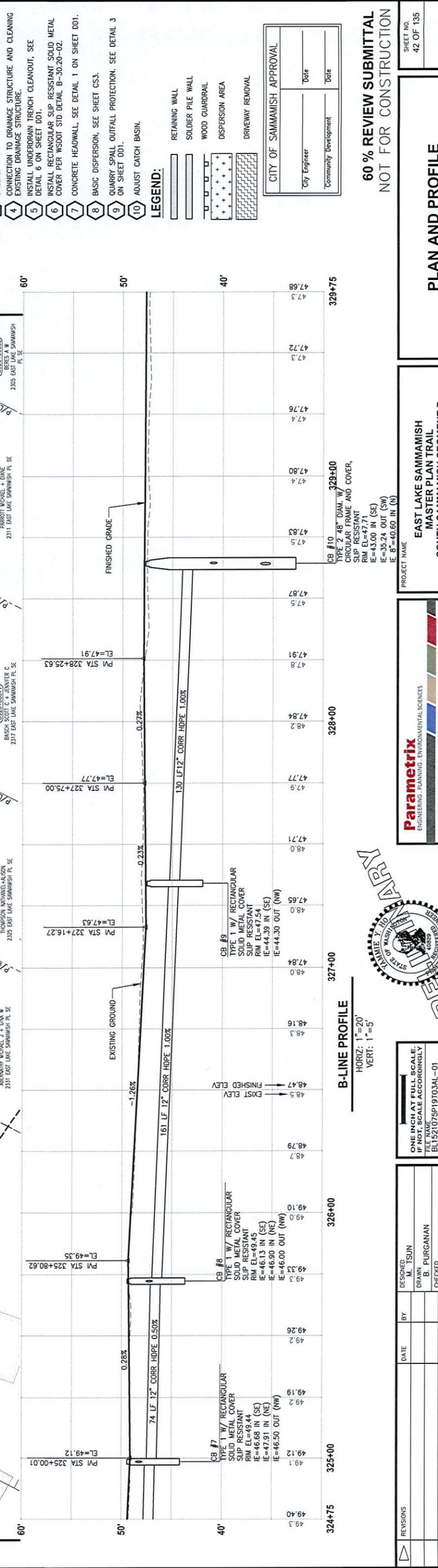
PROJECT NAME

PARAMETRIX
 ENGINEERING, PLANNING, ENVIRONMENTAL SCIENCES
 719 2ND AVENUE, SUITE 200 | SEATTLE, WA 98104
 P. 206.394.3700
 WWW.PARAMETRIX.COM

DESIGNED BY: M. TSUJIN
 DRAWN BY: B. PURGANAN
 CHECKED BY: P. JOHANNESSEN
 APPROVED BY: Y. HO
 DATE: _____
 REVISIONS: _____

ONE INCH AT FULL SCALE.
 IF NOT, SCALE ACCORDINGLY.
 FILE NAME: BL1521075P19T03AL-01
 USER: Y. HO
 PLOT DATE: 5/5/21 15:21-075 P19 T03
 DATE: SEPTEMBER 2016

EXHIBIT 3



PLANNING

PREPARED

APPROVED

DATE

BY

REVISIONS

Exhibit H

CIVIL CONSTRUCTION NOTES:

- RESTORE AND INSTALL STAIR, TYPE TO BE DETERMINED.
- RECONSTRUCT PEDESTRIAN BRIDGE.
- INSTALL PRECAST REINFC CONCR SPLIT BOX CULVERT. SEE FP SHEETS FOR DETAILS.
- INSTALL STRUCTURAL EARTH WALL. SEE WALL DETAILS ON SHEET X. SEE WALL PROFILES ON WP SHEETS.
- INSTALL COATED CHAIN LINK FENCE TYPE 6, FOR GROUND POST INSTALLATION SEE APPENDIX B ON THE SPECIFICATIONS. SEE SHEET X FOR MOUNTING ON TOP OF WALL.
- NEW DRIVEWAY.
- INSTALL WOOD GUARDRAIL, SEE DETAILS ON SHEET XX.
- INSTALL SPLIT RAIL FENCE, SEE DETAILS ON SHEET XX.
- INSTALL GRAVITY BLOCK WALL. SEE WALL DETAILS ON SHEET WDI. SEE WALL PROFILES ON WP SHEETS.
- DRIVEWAY REMOVAL. SEE WP SHEETS FOR PLANTING PLAN.
- INSTALL PATTERED CONCRETE DRIVEWAY. SEE DETAILS ON SHEET XX.
- INSTALL TYPE 1 REST STOP. SEE DETAILS ON SHEET XX.
- INSTALL TYPE 2 REST STOP. SEE DETAILS ON SHEET XX.
- INSTALL SOLDIER PILE WALL.
- INSTALL GRAVITY BLOCK WALL.
- INSTALL WING WALL, SEE FP SHEETS.
- INSTALL EXTRUDED CURB PER WSDOT STD PLAN F-10.42-00.
- REST STOP PULL-OUT.
- STAIRS WILL BE ELIMINATED DURING CONSTRUCTION. BRIDGES WILL BE REMOVED TO R/W LINE DURING CONSTRUCTION.
- INSTALL ROCK WALL.

STORMWATER CONSTRUCTION NOTES:

- INFILTRATION TRENCH, SEE DETAIL 4 ON SHEET DD1.
- UNDERDRAIN TRENCH, SEE DETAIL 5 ON SHEET DD1.
- ROCK PAD, SEE DETAIL 2 ON SHEET DD1 AND WP SHEETS.
- CONNECTION TO DRAINAGE STRUCTURE AND CLEANING EXISTING DRAINAGE STRUCTURE.
- INSTALL UNDERDRAIN TRENCH CLEANOUT, SEE DETAIL 6 ON SHEET DD1.
- INSTALL RECTANGULAR SLIP RESISTANT SOLID METAL COVER PER WSDOT STD DETAIL B-30.20-02.
- CONCRETE HEADWALL, SEE DETAIL 1 ON SHEET DD1.
- BASIC DISPERSION, SEE SHEET CS3.
- QUARRY SPALL OUTFALL PROTECTION. SEE DETAIL 3 ON SHEET DD1.
- ADJUST CATCH BASIN.

LEGEND:

- RETAINING WALL
- SOLDIER PILE WALL
- WOOD GUARDRAIL
- DISPERSION AREA
- DRIVEWAY REMOVAL

CITY OF SAMMAMISH APPROVAL

City Engineer _____ Date _____
 Community Development _____ Title _____

60% REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

PLAN AND PROFILE

EAST LAKE SAMMAMISH
 MASTER PLAN TRAIL
 SOUTH SAMMAMISH SEGMENT B
 SAMMAMISH, WA

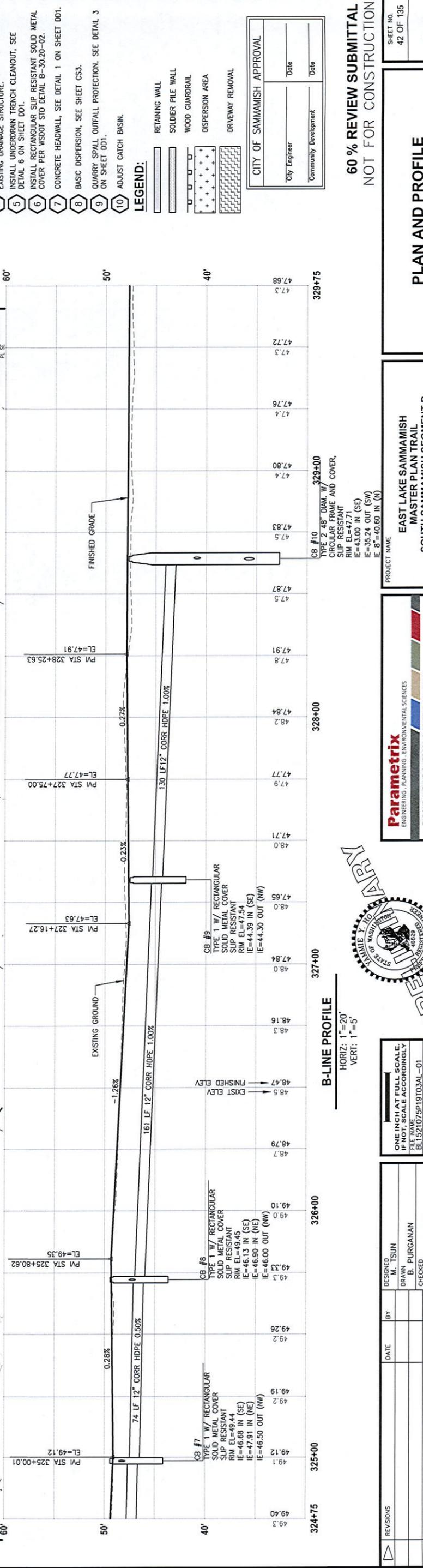
PROJECT NAME
 PROJECT NO. 42 OF 135
 AL-10

DESIGNED BY M. TSUJIN
 DRAWN BY B. PURGANAN
 CHECKED BY P. JOHANNESSEN
 APPROVED BY Y. HO
 DATE _____
 REVISIONS _____

ONE INCH AT FULL SCALE, IF NOT, SCALE ACCORDINGLY
 FILE NAME: BL1521075P19103AL-01
 DATE: 554-1521-075 P19 T03
 SEPTEMBER 2016

PARAMETRIX
 ENGINEERING, PLANNING, ENVIRONMENTAL SCIENCES
 719 2ND AVENUE, SUITE 200 | SEATTLE, WA 98104
 P 206 994 3700
 WWW.PARAMETRIX.COM

STATE OF WASHINGTON
 PROFESSIONAL ENGINEER
 Y. HO
 LICENSE NO. 100000000



- CONSTRUCTION NOTES:**
- REMOVE LAWN IN THIS AREA.
 - REMOVE CONCRETE PAD.
 - REMOVE GRAVEL PAVING.
 - REMOVE STRUCTURE IN THIS AREA.
 - PROTECT EXISTING NATIVE TREES AND SHRUBS; REMOVE BLACKBERRY AND OTHER UNWANTED INVASIVE PLANTS; AMEND SOIL WITH COMPOST; PLANT WITH NATIVE WETLAND PLANTS AND PLACE WOOD CHIP MULCH OVER ENTIRE AREA.
 - PROTECT EXISTING NATIVE TREES AND SHRUBS; REMOVE BLACKBERRY AND OTHER UNWANTED INVASIVE PLANTS; AMEND SOIL WITH COMPOST; PLANT WITH NATIVE BUFFER PLANTS AND PLACE WOOD CHIP MULCH OVER ENTIRE AREA.
 - GRADE AREA TO CREATE WETLAND CONDITIONS AND WETLAND SOIL WITH COMPOST; PLANT WITH NATIVE WETLAND PLANTS.

- GENERAL NOTE:**
- SEE SHEET LA23 FOR DETAILS AND PLANT LISTS.
 - SEE MITIGATION FOR REMOVED DRIVEWAYS IS NOT PART OF THE MITIGATION PLAN.
- LEGEND:**
- WBE WETLAND BUFFER ENHANCEMENT (6)
 - WBA WETLAND BUFFER ADDITION AREA (6)
 - SSE SHORELINE SETBACK ENHANCEMENT AREA (6)
 - WCWR WETLAND CREATION OR RESTORATION AREA (7)
 - WE+ WETLAND ENHANCEMENT AREA (5)
 - SBE STREAM BUFFER ENHANCEMENT AREA (6)
 - SEEDING FOR REMOVED DRIVEWAYS. NOTE 2.

CITY OF SAMMAMISH APPROVAL	
City Engineer	Date
Community Development	Date

60 % REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

SHEET NO.
118 OF 135
LA6

LANDSCAPE PLAN

EAST LAKE SAMMAMISH MASTER PLAN TRAIL SOUTH SAMMAMISH SEGMENT B
SAMMAMISH, WA

Parametrix
ENGINEERING, PLANNING, ENVIRONMENTAL SCIENCES
719 2ND AVENUE, SUITE 200 | SEATTLE, WA 98104
WWW.PARAMETRIX.COM

STATE OF WASHINGTON
LANDSCAPE ARCHITECT
JENS SWENSON
LICENSE NO. 656
EXPIRES ON

DESIGNED BY	J. SWENSON
DRAWN BY	B. PURGANAN
CHECKED BY	P. JOHANNESSEN
APPROVED BY	Y. HO

REVISIONS	DATE	BY

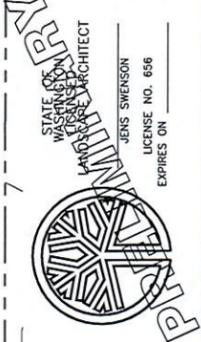
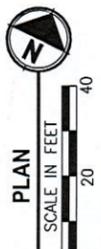
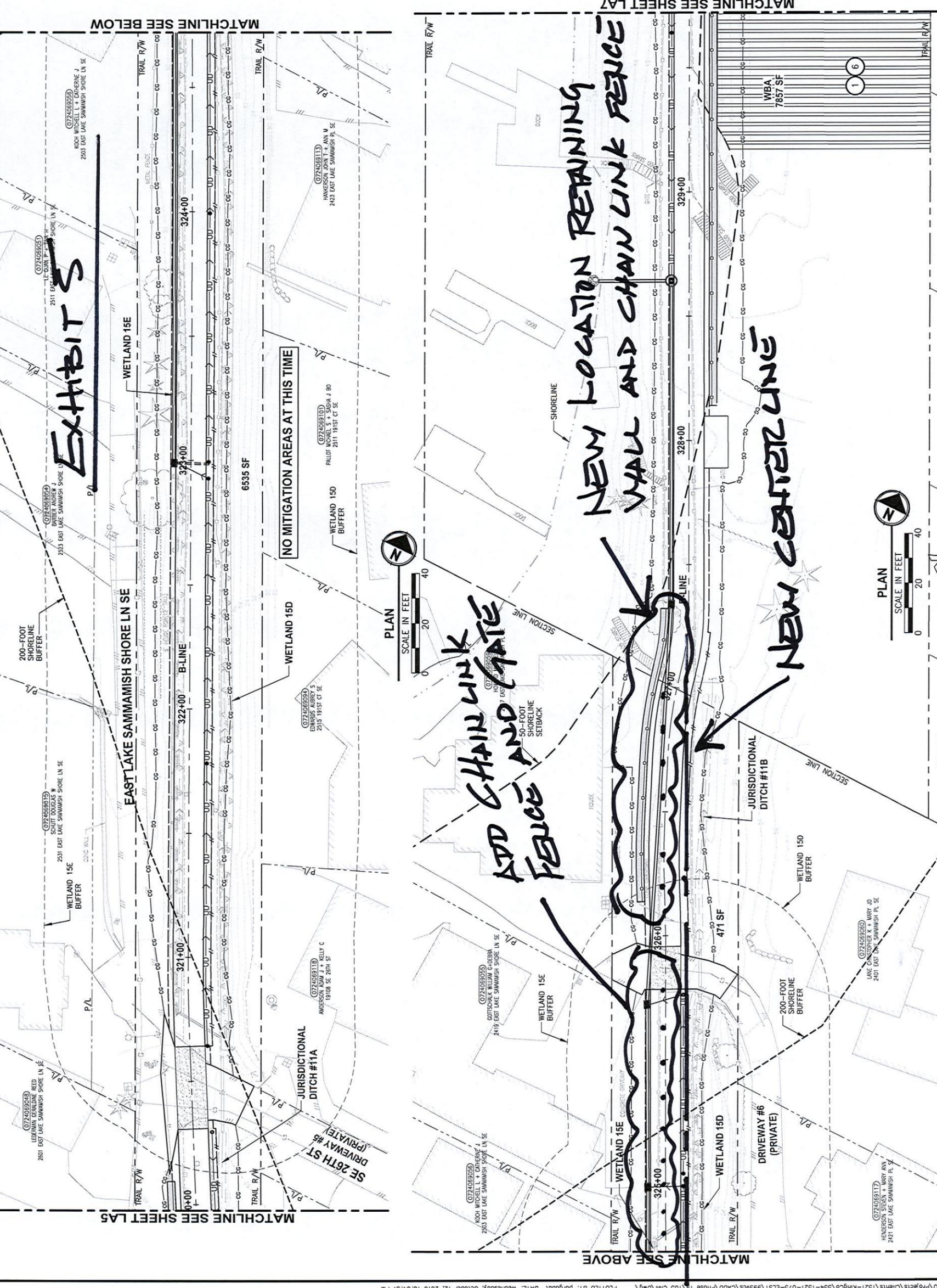


Exhibit 6



PRODUCTS

MARKETS

START A PROJECT

KNOWLEDGE CENTER

COMPANY

Products

Pipe

Corrugated Metal (CMP)

Slotted Drain



Slotted Drain™

Slotted Drain pipe removes sheet flow from streets, highways, and parking lots without multiple grades or water channeling devices. The result is an aesthetically pleasing inlet that is safer and easier to install and maintain.

Re: Please Approve the Permit for Segment 2B of the ELST

Lindsey Ozbolt

Mon 2/6/2017 12:49 PM

To: chasbuit@yahoo.com <chasbuit@yahoo.com>;

Dear Charles,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development

425.295.0527

From: Charles Buitron <chasbuit@yahoo.com>

Sent: Saturday, January 28, 2017 8:32 AM

To: Lindsey Ozbolt

Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the permit, as submitted.

Approval of the permit will advance completion of the 44 mile regional trail system between Seattle and the foothills of the Cascades. The trail, as proposed in the permit, will provide a safe walking and biking route through Sammamish. Please support the proposed trail widths, which reflect industry standards (AASHTO).

A 12ft trail with 2ft shoulders will create a safe trail with space for the various different uses... from people running to people riding a bike. Please approve the permit, including the proposed width of the trail.

Ensuring crossing priority for the trail is an important safety issue. Giving priority to the trail when roads and driveways cross the path will be intuitive for all users. The trail alignment, as proposed in the permit, provides sight lines for good visibility for people on the trail and people crossing the trail at trail intersections. This is my personal response. In today's world of increased stress people need more than ever trails like this to decompress. These resources are disappearing in our growing region. I plan on retiring in Sammamish because of your richness of recreational and natural resources like the ELST. You've done an incredible job in defining your city as a haven. Don't let the selfish act of a few deprive so many of the benefit this trail would provide.

Please approve the permit, as proposed, with expediency.

Sincerely,

Charles Buitron
723 N 50th St
Seattle, WA 98103
206 547 8761

Re: Please Approve the Permit for Segment 2B of the ELST

Lindsey Ozbolt

Mon 2/6/2017 12:50 PM

To: dorainer@gmail.com <dorainer@gmail.com>;

Dear Doraine,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development

425.295.0527

From: Doraine Raichart <dorainer@gmail.com>

Sent: Saturday, January 28, 2017 12:28 PM

To: Lindsey Ozbolt

Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear City of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the permit, as submitted.

Approval of the permit will advance completion of the 44-mile regional trail system between Seattle and the foothills of the Cascades. The trail, as proposed in the permit, will provide a safe walking and biking route through Sammamish. Please support the proposed trail widths, which reflect industry standards (AASHTO).

A 12ft trail with 2ft shoulders will create a safe trail with space for the various different uses of the trail... from running to riding a bike. Please approve the permit with the trail widths as proposed.

Ensuring crossing priority for the trail is an important safety issue. Giving priority to the trail when roads and driveways cross the path will be intuitive for all users, whether in a vehicle, on foot, or on a bike. The trail alignment, as proposed in the permit, provides sight lines for good approach visibility for people on the trail and people crossing the trail.

We cyclists love this loop for our outdoor fitness endeavors and it would be such a jewel for the neighborhood to complete this trail and get us off the road where motorists currently must watch for us. The completed trail will bring lots of hungry cyclists and pedestrians through who will often stop for lunch or a snack, spending money at the local businesses.

Please approve the permit, as proposed, with expediency.

Doraine Raichart
12702 NE Hollyhills Dr
Bothell, WA 98011
2088696209

Re: Automatic reply: Please Approve the Permit for Segment 2B of the ELST

Ernie Priestley <ernie_priestley@yahoo.com>

Sat 1/28/2017 3:50 PM

To:Lindsey Ozbolt <LOzbolt@sammamish.us>;

No need to respond. Just vote to complete the trail.

From: Lindsey Ozbolt <LOzbolt@sammamish.us>
To: Ernest Priestley <ernie_priestley@yahoo.com>
Sent: Saturday, January 28, 2017 3:47 PM
Subject: Automatic reply: Please Approve the Permit for Segment 2B of the ELST

Thank you for your email. I am experiencing a high volume of emails currently and it may take me longer than 24 hours to respond. Additionally, I will be out of the office on Monday, January 30th, attending a mandatory training. I will respond as expeditiously as possible.

Thank you for your understanding.

Best,

Lindsey Ozbolt

Re: Please Approve the Permit for Segment 2B of the ELST

Lindsey Ozbolt

Mon 2/6/2017 12:49 PM

To: jgcookjr@gmail.com <jgcookjr@gmail.com>;

Dear Jonathan,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development

425.295.0527

From: Jonathan Cook <jgcookjr@gmail.com>

Sent: Saturday, January 28, 2017 6:41 AM

To: Lindsey Ozbolt

Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the trail permit, as submitted, so that users of all ages and abilities can safely use the trail. A trail built to national standards (AASHTO), that is 12 ft, plus 2 ft gravel shoulders, will allow for safe use by a variety of different users, including people who walk and bike.

As proposed in the permit, priority at trail crossings should be given to the trail and trail users. Consistent crossing priority is intuitive and safe for users of both the trail and the driveways and roads that cross the trail.

When complete, the trail will be an even greater community amenity, and provide a safe option for people who bike to travel to and through Sammamish. Please complete the trail.

Sincerely,

Jonathan Cook

5555 14th Ave NW, 633

Seattle, WA 98107
8472078910

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Monday, February 6, 2017 12:54 PM
To: scanlon.jonathan@gmail.com
Subject: Re: Please Approve the Permit for Segment 2B of the ELST

Dear Jonathan,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: Jonathan Scanlon <scanlon.jonathan@gmail.com>
Sent: Saturday, January 28, 2017 9:38 PM
To: Lindsey Ozbolt
Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the trail permit, as submitted, so that users of all ages and abilities can safely use the trail. A trail built to national standards (AASHTO), that is 12 ft, plus 2 ft gravel shoulders, will allow for safe use by a variety of different users, including people who walk and bike.

As proposed in the permit, priority at trail crossings should be given to the trail and trail users. Consistent crossing priority is intuitive and safe for users of both the trail and the driveways and roads that cross the trail.

When complete, the trail will be an even greater community amenity, and provide a safe option for people who bike to travel to and through Sammamish. Please complete the trail.

I love riding around Lake Sammamish, but the roads aren't safe and a gravel trail is not best for all. Please complete the high quality trail.

Sincerely,

Jonathan Scanlon
2631 Mayfair Ave N
Seattle, WA 98108
2069253119

Re: Please Approve the Permit for Segment 2B of the ELST

Lindsey Ozbolt

Mon 2/6/2017 12:48 PM

To: kenturner1@verizon.net <kenturner1@verizon.net>;

Dear Kenneth,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: Kenneth Turner <kenturner1@verizon.net>

Sent: Saturday, January 28, 2017 5:07 AM

To: Lindsey Ozbolt

Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the permit, as submitted.

Approval of the permit will advance completion of the 44 mile regional trail system between Seattle and the foothills of the Cascades. The trail, as proposed in the permit, will provide a safe walking and biking route through Sammamish. Please support the proposed trail widths, which reflect industry standards (AASHTO).

A 12ft trail with 2ft shoulders will create a safe trail with space for the various different uses of the trail... from running to riding a bike. Please approve the permit with the trail widths as proposed.

Ensuring crossing priority for the trail is an important safety issue. Giving priority to the trail when roads and driveways cross the path will be intuitive for all users, whether in a vehicle, on foot, or on a bike. The trail alignment, as proposed in the permit, provides sight lines for good approach visibility for people on the trail and people crossing the trail.

On a personal note, someone in my family of five uses the trail daily for either biking, walking, running or dog walking. My daughter and I are training for running and triathlon events and are on bikes that require appropriate surface and safety measures. I work for Microsoft and am part of a large group of riders that communicate on Yammer. I can say that every year

the comments regarding close encounters, encounters, and deaths from car related crashes is increasing. We are so fortunate to have this ability to create a safe environment for the Sammamish and surrounding area biking community.

Please approve the permit, as proposed, with expediency.

Kenneth Turner
216 209th PL SE
Sammamish, WA 98074
443-417-7918

Re: Please Approve the Permit for Segment 2B of the ELST

Lindsey Ozbolt

Mon 2/6/2017 12:49 PM

To: mark.trotter@live.com <mark.trotter@live.com>;

Dear Mark,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development

425.295.0527

From: Mark Trotter <mark.trotter@live.com>

Sent: Saturday, January 28, 2017 8:30 AM

To: Lindsey Ozbolt

Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear City Council for the City of Sammamish,

As a 60-year local resident and regular user of the Lake Sammamish trails, I'm writing to express my avid support for completing the ELST and approving permit SSDP2016-00415.

In short, please approve the permit, as submitted, for completion of the trail.

I regularly walk and bike the area and have had to use East Lake Sammamish Parkway during multiple trail closures or through the areas that are narrow, gravel, or currently unsuitable for road bicycle travel. East Lake Sammamish Parkway is dangerous and, in places, difficult.

The trail is a regional asset and a major safety improvement for the public. It does not represent a significant detriment to the property owners in the area - who are selfishly trying to eliminate or reduce others' use of the trail, which they knew was the long-standing plan when they purchased the properties and the railroad right-of-way had been in place for many years.

I agree that the approval of the permit will advance completion of the 44 mile regional trail system between Seattle and the foothills of the Cascades. The trail, as proposed in the permit, will provide a safe walking and biking route through Sammamish. I add my wholehearted request that you please support the proposed trail widths, which reflect industry standards (AASHTO).

A 12ft trail with 2ft shoulders will create a safe trail with space for the various different uses of the trail... from running to riding a

bike. Please approve the permit with the trail widths as proposed.

Ensuring crossing priority for the trail is an important safety issue. Giving priority to the trail when roads and driveways cross the path will be intuitive for all users, whether in a vehicle, on foot, or on a bike. The trail alignment, as proposed in the permit, provides sight lines for good approach visibility for people on the trail and people crossing the trail.

Thank you for your consideration for my request that you please approve the permit, as proposed, with expediency.

Mark Trotter
16135 NE 113TH CT
Redmond, WA 98052
2067786770

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Monday, February 6, 2017 12:53 PM
To: Peggy Michael Reddy
Subject: Re: see you on Tuesday

Dear Peggy,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: Peggy Michael Reddy <reddy@benefits-consulting.com>
Sent: Saturday, January 28, 2017 3:15 PM
To: 'ELST Master Plan'; Lindsey Ozbolt
Cc: 'Mike Schmidt'; 'Gene Beall'; 'Adam Eaton'
Subject: see you on Tuesday

Dear Kelly: At your request with regard to "Wetland 23C, we called the Corps with disappointing results. I sent an email to Kathy Curry the Corps and called Kathy asking the Corps to review and to consider re-categorizing of "Wetland 23C". Kathy returned my call and indicated that they were NOT reviewing this Wetland and that they are NOT reviewing the trail plan except certain "jurisdictional" boundaries, whatever that means. Kathy made it very clear that we have NO authorization to ask the Corps to review the "Wetland 23C". In fact, Kathy asked us to contact the County (or Jeff Meyer of Parametrix) to address this issue. So we are back to you. Who has the power, knowledge, and information as to what needs to be done to give this Section 23C a review?

Also, I understand from the Corps that it is up to the County to mitigate any impacts to "Wetland 23C" if the trail location remains in its current location, rather than to move it further west. Who has knowledge of the process we need to follow to address our request to keep the trail in its current location and to mitigate any impacts?

No more wild goose chases please!!!

I accept your invitation to meet me at my property on Tuesday the 31st at 9:00 a.m. to address the County stakes on my property before I remove them. Attached again are both the boundary line adjustment of 1999 and the Site Survey. The Quit Claim Deed was recorded at King County. Please contact the County recording office for the official document.

Thank you, Peggy

-----Original Message-----

From: Curry, Kathryn E CIV USARMY CENWS (US) [mailto:Kathryn.E.Curry@usace.army.mil]
Sent: Friday, January 27, 2017 9:39 AM
To: Peggy Michael Reddy <reddy@benefits-consulting.com>; 'ELST Master Plan' <ELST@kingcounty.gov>; lozbolt@sammamish.us
Cc: 'Karrah Penk (Benefits Consulting Services LLC)' <karrah@benefits-consulting.com>
Subject: RE: [EXTERNAL] RE: East Lake Sammamish Trail Segment B Wetlands 23C

Peggy,

I can only relay that we have not been requested to review Wetland 23C at this time. On wetlands we have been asked to review, at this time we are only looking at whether or not they are jurisdictional (regulated) wetlands, streams or ditches, and in some cases we are also looking at the location of jurisdictional wetland, stream or ditch boundaries. We are not reviewing or commenting on project design, location, etc.

Where the project design impacts jurisdictional features, I expect that the applicant will be, at some time in the future, submitting documentation to the Corps for a nationwide permit and providing mitigation for impacts to jurisdictional features.

Regards, Kathy

Kathryn E. Curry, PWS
Regulatory Branch, Seattle District
USACE
206-764-5527
Kathryn.E.Curry@usace.army.mil

-----Original Message-----

From: Peggy Michael Reddy [mailto:reddy@benefits-consulting.com]
Sent: Friday, January 27, 2017 9:24 AM
To: Curry, Kathryn E CIV USARMY CENWS (US) <Kathryn.E.Curry@usace.army.mil>; 'ELST Master Plan' <ELST@kingcounty.gov>; lozbolt@sammamish.us
Cc: 'Karrah Penk (Benefits Consulting Services LLC)' <karrah@benefits-consulting.com>
Subject: RE: [EXTERNAL] RE: East Lake Sammamish Trail Segment B Wetlands 23C

Very odd..thanks but they specifically told us to contact you.

-----Original Message-----

From: Curry, Kathryn E CIV USARMY CENWS (US) [mailto:Kathryn.E.Curry@usace.army.mil]
Sent: Friday, January 27, 2017 9:22 AM
To: Peggy Michael Reddy <reddy@benefits-consulting.com>
Cc: Karrah Penk (Benefits Consulting Services LLC) <karrah@benefits-consulting.com>
Subject: RE: [EXTERNAL] RE: East Lake Sammamish Trail Segment B Wetlands 23C

Peggy,

Thank you for your email. As I have relayed to Mike Schmidt, Wetland 23 C is not currently part of our review scope. I encourage you to engage with the County and City regarding your concerns about the project design.

Regards, Kathy

Kathryn E. Curry, PWS
Regulatory Branch, Seattle District
USACE
206-764-5527
Kathryn.E.Curry@usace.army.mil

-----Original Message-----

From: Peggy Michael Reddy [mailto:reddy@benefits-consulting.com]
Sent: Friday, January 27, 2017 7:45 AM
To: Curry, Kathryn E CIV USARMY CENWS (US) <Kathryn.E.Curry@usace.army.mil>
Cc: Karrah Penk (Benefits Consulting Services LLC) <karrah@benefits-consulting.com>
Subject: [EXTERNAL] RE: East Lake Sammamish Trail Segment B Wetlands 23C

Hi Kathy: My name is Peggy Reddy. I'm a property owner adjacent to the proposed trail at the location referenced by my neighbor Mike. Attached are my comments to the County and City.

In follow-up Mike's comments we believe the designation of the Section 23C as a wetland may not be correctly categorized and has very negative consequences fully described in my impassioned appeal. We appreciate your review and your reconsideration of Section 23C's "wetland" designation. If, after review by the USACE, it still remains a "wetland" designation please advise what authorizations and opportunities we have to be granted an exception. Thank you for your time.

Peggy

Peggy Reddy

929 ELS Shore Lane SE

Sammamish, WA 98075

206.484.14845

From: Mike Schmidt
[mailto:IMCEAEX-_O=FIRST+20ORGANIZATION_OU=EXCHANGE+20ADMINISTRATIVE+20GROUP
+28FYDIBOHF23SPDLT+29_CN=RECIPIENTS_CN=0002010000008164@eop-nam02.prod.p
+28FYDIBOHF23SPDLT+rote
ction.outlook.com]

Sent: Thursday, January 26, 2017 6:02 PM
To: Kathryn.E.Curry@usace.army.mil
Subject: East Lake Sammamish Trail Segment B Wetlands 23C

Hi Kathy, my name is Mike Schmidt and I am a resident in Sammamish along the Sammamish Trail Segment 2B near station 380. This week I provided feedback regarding details of the proposed Sammamish Trail Segment B plans in our neighborhood to Lindsey Ozbolt and Kelly Donahue, and Kelly suggested that I could contact you regarding the disposition of the habitat in the trail ROW in our neighborhood. I have also included the feedback I sent to the City/County in the attached email if you would like additional context.

My understanding is that you are in the process of reevaluating the Trail Segment 2B area, and in particular evaluating what areas are considered wetlands. I was very pleased to hear this, and I would like to draw your attention in particular to Wetland 23C located near station 378 on sheet AL20. My concern with this area's designation as a wetland is for two reasons:

1) To the untrained eye it does not look like wetlands, nor does there appear to be any wetland flora in the area. It is effectively a blackberry covered hill sloping away from the Sammamish Parkway that ends in a drain ditch at the east edge of the current trail. Besides the previously mentioned blackberries there are also tall grasses and a few scraggly trees in the area. When I compare this area to the area directly south of it (section 376) that is not currently designated as wetlands the soil composition and plants look quite similar, with the possible exception that the area further south has more trees as you continue south. In any case, since you are reevaluating this area that gives me some hope that the current designation in the trail plans might be erroneous, which leads me to my second point.

2) It is my understanding that the current designation of this area as Wetland 23C may have caused the design for the new path of the trail to divert to the west of the current interim trail, away from currently designated Wetland 23C. Although preservation of wetlands (as currently designated) is understandable, this has the terrible side effect of wiping out over 150 feet of beautiful landscaping which includes 4 mature Aspen trees and 5 mature fir trees, in addition to a host of mature Rhododendrons, Oregon Grape, and other plants. Just standing there on the trail and looking down it at either side, it becomes very clear which part should be preserved and which should be used for the trail bed.

I hope that as part of evaluating the area you will keep this feedback in mind, and hope that both the determination of wetlands can be changed, as well as hopefully redirecting the trail back to the east closer to following the current trail bed as it does just south of this area at segment 377.

This would allow the preservation of the highly desirable plants and mature trees in this area.

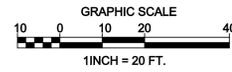
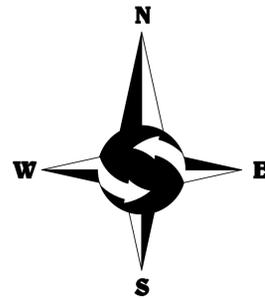
Thank you for your consideration, and please let me know if I may provide any further clarification or if you would like to meet in person at the site to discuss this further.

--Mike Schmidt

903 East Lake Sammamish Shore Lane SE

Sammamish, WA 98075

425 836 3259



LEGAL DESCRIPTION

LOT A OF KING COUNTY BOUNDARY LINE ADJUSTMENT NO L99L3056, AS RECORDED IN VOLUME 133, PAGE 139, UNDER RECORDING NUMBER 19991101900003, RECORDS OF KING COUNTY, WASHINGTON.

SITUATE IN THE CITY OF SAMMAMISH, COUNTY OF KING, STATE OF WASHINGTON

BASIS OF BEARINGS

KING COUNTY BOUNDARY LINE ADJUSTMENT NO L99L3056, AS RECORDED IN VOLUME 133, PAGE 139, UNDER RECORDING NUMBER 19991101900003, RECORDS OF KING COUNTY, WASHINGTON. ACCEPTED RECORD BEARINGS BASED ON FOUND MONUMENTS.

GENERAL NOTES

1. THIS SURVEY WAS COMPLETED WITHOUT BENEFIT OF A CURRENT TITLE REPORT. EASEMENTS AND OTHER ENCUMBRANCES MAY EXIST ON THIS PROPERTY THAT ARE NOT SHOWN HEREON.
2. INSTRUMENTATION FOR THIS SURVEY WAS A 3-SECOND NIKON NPL 362 TOTAL STATION. PROCEDURES USED IN THIS SURVEY MEET OR EXCEED STANDARDS SET BY WAC 332-130-090.
3. THE INFORMATION ON THIS MAP REPRESENTS THE RESULTS OF A SURVEY MADE IN JANUARY 2013 AND CAN ONLY BE CONSIDERED AS INDICATING THE GENERAL CONDITIONS EXISTING AT THAT TIME.
4. UTILITIES SHOWN ON THIS SURVEY ARE BASED UPON ABOVE GROUND OBSERVATIONS AND AS-BUILT PLANS WHERE AVAILABLE. ACTUAL LOCATIONS OF UNDERGROUND UTILITIES MAY VARY AND UTILITIES NOT SHOWN ON THIS SURVEY MAY EXIST ON THIS SITE.
5. ALL MONUMENTS WERE LOCATED DURING THIS SURVEY UNLESS OTHERWISE NOTED.

FLOOD ZONE DESIGNATION

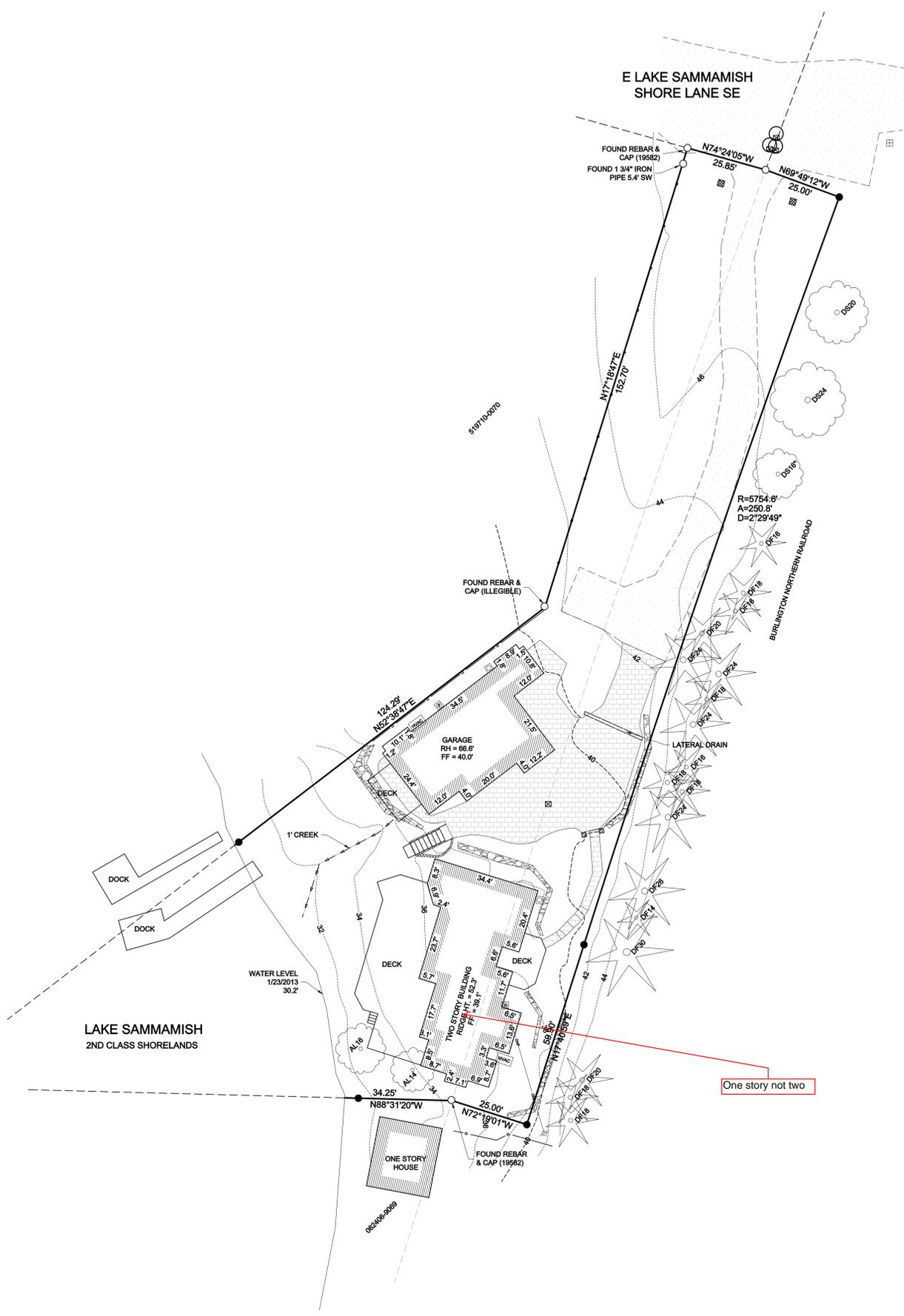
FLOOD ZONE DESIGNATION = X, AREA DETERMINED TO BE OUTSIDE OF 500-YEAR FLOOD PLAIN, ACCORDING TO FLOOD INSURANCE RATE MAP (FIRM) NO. 53033C0680 F, COMMUNITY NO. 530074 (UNINCORPORATED KING COUNTY), PANEL 0880, SUFFIX F, REVISED MAY 16, 1995, KING COUNTY, WASHINGTON, AS PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA). A PORTION OF THE LOT IS ALSO WITHIN FLOOD ZONE DESIGNATION = AE (BFE 33' NGVD 29 - 36.59' NAVD 88)

VERTICAL DATUM & CONTOUR INTERVAL

ELEVATIONS SHOWN ON THIS DRAWING WERE DERIVED FROM INFORMATION PROVIDED BY WCCS SURVEY CONTROL DATABASE.

POINT ID NO. 2355
REBAR & 1" ALUMINUM CAP ON THE EAST SHOULDER OF E LAKE SAMMAMISH PARKWAY, SOUTH OF THE INTERSECTION WITH EAST LAKE SAMMAMISH SHORES.
ELEVATION: 59.357 FEET (18.092 METERS) NAVD 88

2.0' CONTOUR INTERVAL - THE EXPECTED VERTICAL ACCURACY IS EQUAL TO 1/2 THE CONTOUR INTERVAL OR PLUS / MINUS 1.0' FOR THIS PROJECT.



LEGEND

- FOUND MONUMENT AS DESCRIBED
- FOUND REBAR AS DESCRIBED
- SET 5/8" X 24" IRON ROD W/1" YELLOW PLASTIC CAP
- ⊠ POWER METER
- ⊕ UTILITY POLE
- ⊖ GUY WIRE
- ⊗ TELECOMMUNICATION 150' MONOPOLE
- ⊙ SANITARY SEWER MANHOLE
- ⊙ STORM DRAIN MANHOLE
- ⊠ CATCH BASIN
- ⊠ HVAC UNIT
- ⊠ UTILITY VAULT
- ⊙ SEPTIC TANK LID
- ⊠ MAILBOX
- ⊠ GAS METER
- ⊠ LIGHT VAULT
- ⊠ LIGHT POLE
- ⊠ WATER METER
- CLEAN OUT
- ⊠ STREET SIGN
- ⊠ FIRE HYDRANT
- ⊠ WOOD FENCE
- ⊠ CONCRETE WALL
- ⊠ BRICK SURFACE
- ⊠ CONCRETE SURFACE
- ⊠ GRAVEL SURFACE
- ⊠ ROCKERY
- AL ALDER
- DF DOUGLAS FIR
- DS DECIDUOUS
- * DENOTES MULTI TRUNK

PROJECT INFORMATION

SURVEYOR: SITE SURVEY & MAPPING, INC.
10115 214TH AVE NE
REDMOND, WA 98053
PHONE: 425.298.4412

PROPERTY OWNER: PEGGY REDDY
929 E LAKE SAMMAMISH SHORE LANE SE
SAMMAMISH, WA 98075

TAX PARCEL NUMBER: 519710-0075

PROJECT ADDRESS: 929 E LAKE SAMMAMISH SHORE LANE SE
SAMMAMISH, WA 98075

ZONING: R-4

JURISDICTION: CITY OF SAMMAMISH

PARCEL ACREAGE: 20,500 S.F. (± 0.471 ACRES)
AS SURVEYED, UPLANDS OF HIGH WATER.

NE 1/4, NE 1/4, SEC 06, TWP 24N, RNG 6E, W.M.



DATE	REVISION	DRN

TOPOGRAPHIC SURVEY
PEGGY REDDY
929 E LAKE SAMMAMISH SHORE LANE SE
SAMMAMISH, WA 98075

PROJECT NO. 12-410

DRAWN BY: EFJ
CHECKED BY: TNW
DATE: 01-23-13

SHEET 1 OF 1

KING COUNTY, WASHINGTON



BOUNDARY LINE ADJUSTMENT NO. E99L3056

DECLARATION

KNOW ALL MEN BY THESE PRESENTS THAT WE THE UNDERSIGNED OWNER(S) OF THE LAND HEREIN DESCRIBED DO HEREBY MAKE A BOUNDARY LINE ADJUSTMENT THEREOF PURSUANT TO RCW 58.17.040 AND DECLARE THIS ADJUSTMENT TO BE THE GRAPHIC REPRESENTATION OF THE SAME, AND THAT SAID ADJUSTMENT IS MADE WITH THE FREE CONSENT AND IN ACCORDANCE WITH THE DESIRES OF THE OWNER(S). IN WITNESS WHEREOF WE HAVE SET OUR HANDS AND SEALS.

James J. Reddy
Name JAMES J. REDDY
Peggy M. Reddy
Name PEGGY M. REDDY

Name _____
Name _____
Name _____
Name _____

State of Washington
County of _____
I certify that I know or have satisfactory evidence that JAMES J. REDDY signed this instrument and acknowledged it to be (his/her) free and voluntary act for the uses and purposes mentioned in the instrument.

Signature of CATHERINE
Notary Public
Dated 10/29/99
My appointment expires 3/19/01

State of Washington
County of KING
I certify that I know or have satisfactory evidence that PEGGY M. REDDY signed this instrument and acknowledged it to be (his/her) free and voluntary act for the uses and purposes mentioned in the instrument.

Signature of CATHERINE
Notary Public
Dated 10/29/99
My appointment expires 3/19/01

APPROVAL

DEPARTMENT OF DEVELOPMENT AND ENVIRONMENTAL SERVICES
RECOMMENDED FOR APPROVAL THIS 20th DAY OF OCTOBER, 1999
Mark R. ...
Manager, Land Use Services Division

KING COUNTY DEPARTMENT OF ASSESSMENTS
Examined and approved this 1st day of NOVEMBER, 1999

SCOTT NOBLE James ...
King County Assessor Deputy King County Assessor
519710-0075 NE 6-24-6

RECORDING NO. _____

RECORDER'S CERTIFICATE

filed for record this 1st day of Nov., 1999 at 2:11 M in book 133 at page 135 at the request of
NEIL CABBAGE
SURVEYOR'S NAME
Bob ... *Walt ...*
Mgr. Supt. of Records

NOTES:

- 1) OLD LOT "A" KNOWN AS TAX LOT 519710-0075
- 2) OLD LOT "B" KNOWN AS TAX LOT 0624069013
- 3) THIS BOUNDARY LINE ADJUSTMENT WAS ACCEPTED, REVIEWED AND APPROVED BY KING COUNTY AS AN ANCILLARY PERMIT TO KING COUNTY BUILDING PERMIT NO. B99A1796

FND. REBAR & CAP S 38° E, 0.4' FROM CALC'D POSITION LS No. 19582

FND. REBAR & CAP S 38° E, 0.4' FROM CALC'D POSITION LS No. 19582

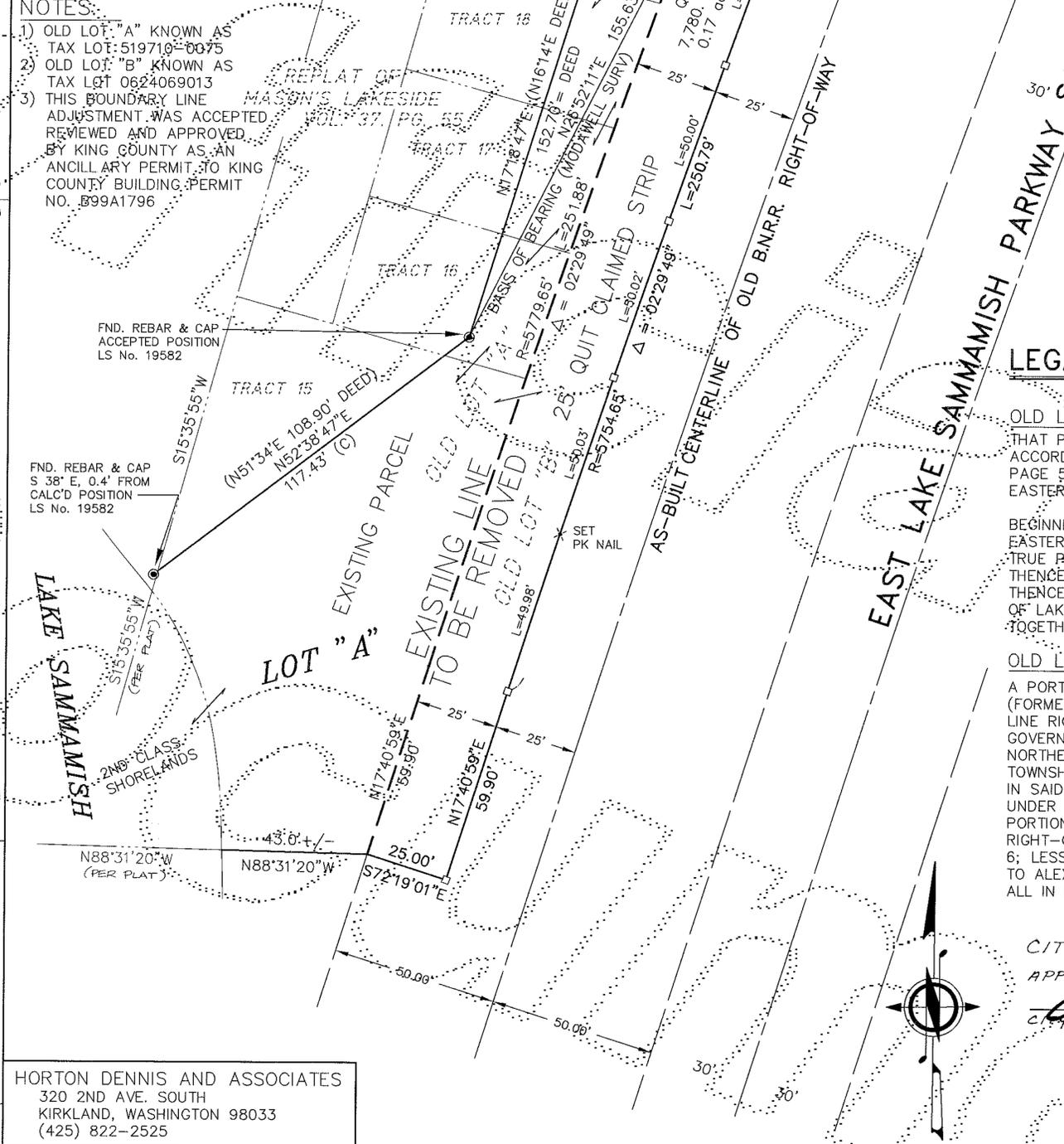
FND. REBAR & CAP S 38° E, 0.4' FROM CALC'D POSITION LS No. 19582

FND. REBAR & CAP S 54° E, 0.6' FROM CALC'D POSITION LS No. 19582

FND. REBAR & CAP ACCEPTED POSITION LS No. 19582

FND. 1.75" IRON PIPE 5.4' FROM CORNER AND 0.1' SOUTHEAST FROM LINE

NORTH LINE TRACT 19 PER MODAWELL SURVEY FOR BEARING AND DIST. PLATTED = 66.87 MEASURED = 67.95 TO AS-BUILT RR CENTERLINE



LEGEND

- SET LINE STAKE W/TACK
- × SET PK NAIL
- FOUND REBAR OR PIPE AS NOTED
- (C) CALCULATED VALUE

BASIS OF BEARINGS AND HDA SURVEY: BASED ON FOUND LOT CORNERS SET BY MODAWELL & ASSOCIATES, PER SURVEY FOR STEVE GRAVES, DATED 6/24/97, JOB NO. 97115.

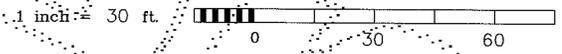
LEGAL DESCRIPTION

OLD LOT "A"
THAT PORTION OF TRACTS 15 THROUGH 19, MASON'S LAKESIDE REPLAT, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 37 OF PLATS, PAGE 55, IN KING COUNTY, WASHINGTON, LYING EASTERLY AND SOUTH-EASTERLY OF A LINE DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF TRACT 19, RUNNING THENCE EASTERLY ALONG THE NORTH LINE OF SAID TRACT, 42.10 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 16° 14' 00" WEST 152.70 FEET; THENCE SOUTH 51° 34' 00" WEST 108.90 FEET TO IRON PIPE ON SHORELINE OF LAKE SAMMAMISH AND ON TRACT LINE BETWEEN TRACTS 14 AND 15; TOGETHER WITH SECOND CLASS SHORELANDS ADJOINING.

OLD LOT "B"
A PORTION OF BURLINGTON NORTHERN AND SANTA FE RAILWAY COMPANY'S (FORMERLY NORTHERN PACIFIC RAILWAY COMPANY) SNOQUALMIE BRANCH LINE RIGHT-OF-WAY BEING 100.00 FEET WIDE OVER AND ACROSS GOVERNMENT LOTS 1, 2, AND 3 AND THE NORTHWEST QUARTER OF THE NORTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 6, TOWNSHIP 24 NORTH, RANGE 6 EAST, W. M.; LESS PORTION THEREOF IN SAID GOVERNMENT LOT 3 AS DEEDED TO PATRICK AND VICKI BURNS UNDER RECORDING NUMBER 9701221277 AND TOGETHER WITH THAT PORTION OF SAID RAILWAY COMPANY'S 200 FOOT WIDE BRANCH LINE RIGHT-OF-WAY OVER AND ACROSS GOVERNMENT LOT 4 IN SAID SECTION 6; LESS PORTION DEEDED FROM NORTHERN PACIFIC RAILWAY COMPANY TO ALEX KOLL UNDER RECORDING NUMBER 2402400. ALL IN KING COUNTY, WASHINGTON

CITY OF SAMMAMISH APPROVALS
APPROVED THIS 25th DAY OF October, 1999
Allen S. ...
CITY ENGINEER CITY OF SAMMAMISH



HORTON DENNIS AND ASSOCIATES
320 2ND AVE. SOUTH
KIRKLAND, WASHINGTON 98033
(425) 822-2525

LAND SURVEYOR'S CERTIFICATE

This BOUNDARY LINE ADJUSTMENT correctly represents a survey made by me or under my direction in conformance with state and county statutes in MAY, 1999

Neil Cabbage
Certificate No. 27345



APPROVAL NOTES:

THIS REQUEST QUALIFIES FOR EXEMPTION UNDER KCC 19.08.112. IT DOES NOT GUARANTEE THAT THE LOTS WILL BE SUITABLE FOR DEVELOPMENT NOW OR IN THE FUTURE. THE LEGAL TRANSFER OF THE PROPERTY MUST BE DONE BY SEPARATE INSTRUMENT, UNLESS ALL PARCELS ARE UNDER THE SAME OWNERSHIP.

PORTION OF:

NE 1/4, NE 1/4, SEC. 6, TWP. 24N., RGE. 6E., W.M.

DWN. BY NSC	DATE 10/8/99	JOB NO. 9920.00
CHKD. BY NSC	SCALE 1"=30'	SHEET 1 OF 1

Re: Please Approve the Permit for Segment 2B of the ELST

Lindsey Ozbolt

Mon 2/6/2017 12:54 PM

To:beersxx@aol.com <beersxx@aol.com>;

Dear Robert,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development

425.295.0527

From: Robert Beers <beersxx@aol.com>

Sent: Saturday, January 28, 2017 11:41 PM

To: Lindsey Ozbolt

Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the trail permit, as submitted, so that users of all ages and abilities can safely use the trail. A trail built to national standards (AASHTO), that is 12 ft, plus 2 ft gravel shoulders, will allow for safe use by a variety of different users, including people who walk and bike.

As proposed in the permit, priority at trail crossings should be given to the trail and trail users. Consistent crossing priority is intuitive and safe for users of both the trail and the driveways and roads that cross the trail.

When complete, the trail will be an even greater community amenity, and provide a safe option for people who bike to travel to and through Sammamish. Please complete the trail.

I often ride Redmond/Issaquah trail but having to go to the highway for that part that is still dirt is a real pain. I have not heard of any complaints of home owners along the completed hard surface part of the trail. Please help provide a safe trail for all riders & walkers.

Sincerely,

Robert Beers
18123 NE 91st Ct
Redmond, WA 98052
206-909-2449

Re: East Lake Sammamish Trail

Richard Lowthian <LowthianRA@AOL.com>

Mon 2/6/2017 9:05 PM

To:Lindsey Ozbolt <LOzbolt@sammamish.us>;

Lindsey,

Thank you for your response to my e-mail and for your consideration of a complete East Lake Sammamish Trail.

Rick

> On Feb 6, 2017, at 12:50 PM, Lindsey Ozbolt <LOzbolt@sammamish.us> wrote:
>
> Dear Richard,
>
> Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).
>
> Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.
>
> Regards,
>
>
> Lindsey Ozbolt
> Associate Planner | City of Sammamish | Department of Community Development
> 425.295.0527
> _____
> From: Richard Lowthian <LowthianRA@AOL.com>
> Sent: Saturday, January 28, 2017 11:18 AM
> To: City Council; Lindsey Ozbolt; Kelly.donahue@kingcounty.gov
> Subject: East Lake Sammamish Trail
>
> Gentle Folk,
>
> Please support the completion and opening of the East Lake Sammamish Trail to the public. The trail system here in King County seems to be one of the best in the country. The East Lake Sammamish Trail contributes greatly to that system. A while back, I set out from my home in Redmond to bicycle to Issaquah seeing that the trail appeared to be open leaving Marymoor Park. After riding most of the way south along the lake, I was greatly disappointed to be detoured off of the trail to the road which was busy with afternoon commute traffic. Although I could have proceeded along the road, the point of the ride - a pleasant and safe outing to be rewarded with a milkshake at the XXX in Issaquah - was lost. I have not been back to this trail since. A trail to nowhere is pointless.
>
> Thank you for your consideration and support for a complete East Lake Sammamish Trail.
>
> Rick Lowthian

Re: Please Approve the Permit for Segment 2B of the ELST

Lindsey Ozbolt

Mon 2/6/2017 12:52 PM

To: Sktothpat@gmail.com <Sktothpat@gmail.com>;

Dear Susan,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Although the official comment period has closed, as of January 27, 2017 at 5:00 p.m., your comments have been received and will be considered.

Regards,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development

425.295.0527

From: Susan Toth Patiejunas <Sktothpat@gmail.com>

Sent: Saturday, January 28, 2017 3:26 PM

To: Lindsey Ozbolt

Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415. I live near the trail and use it to walk and bike. As a resident of Sammamish and a user of the trail, I believe a complete trail benefits the community.

Please approve the trail permit, as submitted, so that users of all ages and abilities can safely use the trail. A trail built to national standards (AASHTO), that is 12 ft, plus gravel shoulders, will allow for safe use by a variety of different users, including people who walk and bike.

As proposed in the permit, priority at trail crossings should be given to the trail and trail users. Consistent crossing priority is intuitive and safe for users of both the trail and the driveways and roads that cross the trail.

When complete, the trail will be an even greater community amenity than in its interim state, and will provide a safe option for people who bike to travel to and through Sammamish. Please complete the trail.

Sincerely,

Susan Toth Patiejunas
1609 205th PI NE
Sammamish, WA 98074
425-241-5531