

RE: Comments on 60% Design 453+61.87 and 454+00

Lindsey Ozbolt

Wed 2/8/2017 1:35 PM

To: Jeff Lum <jefflum1@gmail.com>;

Hi Jeff,

You can check in periodically with me to see if we have received response from the county yet. At this point City staff is still in the preliminary review of the comments and have not transmitted them to King County yet for their review and response.

Best,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

-----Original Message-----

From: Jeff Lum [<mailto:jefflum1@gmail.com>]
Sent: Monday, February 6, 2017 12:42 PM
To: Lindsey Ozbolt <LOzbolt@sammamish.us>
Subject: Re: Comments on 60% Design 453+61.87 and 454+00

Thanks Lindsey. Will I be able to see the response from the county regarding our specific comments?

Jeff

Sent from my iPhone

> On Feb 6, 2017, at 10:18 AM, Lindsey Ozbolt <LOzbolt@sammamish.us> wrote:
>
> Dear Jeff,
>
> Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).
>
> Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.
>
> Regards,
>
> Lindsey Ozbolt
> Associate Planner | City of Sammamish | Department of Community
> Development 425.295.0527?
>
> _____
> From: Jeff Lum <jefflum1@gmail.com>

> Sent: Friday, January 27, 2017 4:31 PM
> To: Lindsey Ozbolt
> Cc: Jill Lum
> Subject: Re: Comments on 60% Design 453+61.87 and 454+00
>
> Liz,
>
> The attached pages will REPLACE the comments sent previously. Please let me know if you have any questions. I appreciate the time we spent on discussing how we could make our previous comments clearer to the county.
>
> Thanks,
> Jeff
>

Comments/questions related to station numbers 454+00 and 453+61.87. We own both properties.

454+00:

1. We will lose about 5-10' of parking depth when the guardrail is constructed east of where it is now. This creates a huge parking problem for us. The parking area is shared by 5 properties. Right now we barely have enough room to park the cars at an angle and have cars get in and out and around each other. The parkway hillside is to the east of the parking area. We'd like to request the county replace the amount of footage we are losing on the trail side of the parking area with an equal amount of footage on the parkway side of the parking area.

The county would need to construct a retaining wall to replace the parking area depth we will lose from the wider trail. If we can't get ample room in the parking area, there are several problems this creates for the homeowners:

A. The homeowners will have to park parallel to the guardrail, which makes it very difficult, if not impossible, to turn around since it is a dead end area.

B. If the homeowners have to park parallel to the guardrail, then that will provide less parking for the 5 owners and their guests, and make for difficult access to their respective properties. The only other available area to park is on the East Lake Sammamish Parkway, which creates a bike lane blockage and a dangerous traffic situation for bikers, automobiles, and people exiting/entering their automobiles.

I have attached a few photos to provide a perspective of what the area looks like with cars parked at today's angle.

2. The 60% plan indicates that the county will be using the driveway to our property and the parking area as a staging area for work near our area and maybe other areas nearby. We'd like clear assurance that:

A. The driveway and parking area will be in as good, or better, condition during and after construction, and

B. That we will have clear and safe access to our property during trail construction.

3. The "CG" appears to extend about 25' west from the back of the current guardrail. The stairway to our property will get demolished. We'd like an understanding as to how we're going to get clear and safe access to our home during construction.

**Exhibit 29
SSDP2016-00415
002476**

4. What flexibility is there in the design/direction of the stairway? Right now it shows a stairway that runs parallel to the trail. Can we have the stairway constructed so that it goes toward our home down to our current landing area? Will the county allow for some flexibility in this design? Since we are losing our current stairway, anyway, can we move the new stairway to originate at a different location than it is now?
5. The 60% plan shows that we are sharing an entrance to our stairway with station 455+00? Based on the answer to #4 above, if we have to share a stairway entrance, we'd prefer to share an entrance with 453+61.87. 455+00 should have a shared stairway with their adjoining station 455+39.35.
6. Will the county will allow us to put our own privacy fence behind, or in place of, the chain link fence?

453+61.87:

1. We will lose about 5-10' of parking depth when the guardrail is constructed east of where it is now. This creates a huge parking problem for us. The parking area is shared by 5 properties. Right now we barely have enough room to park the cars at an angle and have cars get in and out and around each other. The parkway hillside is to the east of the parking area. We'd like to request the county replace the amount of footage we are losing on the trail side of the parking area with an equal amount of footage on the parkway side of the parking area.

The county would need to construct a retaining wall to replace the parking area depth we will lose from the wider trail. If we can't get ample room in the parking area, there are several problems this creates for the homeowners:

A. The homeowners will have to park parallel to the guardrail, which makes it very difficult, if not impossible, to turn around since it is a dead end area.

B. If the homeowners have to park parallel to the guardrail, then that will provide less parking for the 5 owners and their guests, and make for difficult access to their respective properties. The only other available area to park is on the East Lake Sammamish Parkway, which creates a bike lane blockage and a dangerous traffic situation for bikers, automobiles, and people exiting/entering their automobiles.

I have attached a few photos to provide a perspective of what the area looks like with cars parked at today's angle.

2. We would like a separate gate and an access stairway for this parcel. As an alternative, we'd be willing to have a shared entrance and stairway with station 454+00.
3. The 60% plan indicates that the county will be using the driveway to our property and the parking area as a staging area for work near our area and maybe other areas nearby. We'd like some assurance that:
 - A. The driveway and parking area will be in as good, or better, condition during and after construction, and
 - B. That we will have clear and safe access to our property during trail construction.

PHOTOS OF PARKING AREA AS DISCUSSED IN COMMENTS FOR
STATION 454+00 AND STATION 453+61.87

(THIS ALSO APPLIES TO STATIONS 453+00, 455+00 AND 455+39.35)



The distance from the existing guardrail to the bank is 21'. The new guard rail will move towards the bank by at least 5-10 feet (maybe more). Today there is only about 7' between a parked car and the bank – just enough for other vehicles to squeeze by.



The bank is full of invasive blackberry bushes which have been maintained each year by the homeowners in this area. The bank is very deep and tall. The county could design a retaining wall to push back the bank by enough margin to replace the parking that will be lost when the guardrail moves towards the bank.

Exhibit 29

SSDP2016-00415

002479

RE: 1139 E LAKE SAMMAMISH PARKWAY NE

Lindsey Ozbolt

Wed 2/8/2017 1:16 PM

To: john rohrbach <upperstarmeadow@gmail.com>; Snowanh <snowanh@aol.com>;

Cc: David Pyle <DPyle@sammamish.us>;

Good afternoon Mr. Rohrbach,

Gina Auld is the Project Manager for King County Parks on this project and Kelly Donahue, Community Engagement for King County Parks on this project. To contact the County regarding the East Lake Sammamish Trail Segment 2B please call their hotline at 1-888-668-4886 or email them at ELST@kingcounty.gov.

Best,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: john rohrbach [mailto:upperstarmeadow@gmail.com]

Sent: Tuesday, February 7, 2017 5:07 AM

To: Lindsey Ozbolt <LOzbolt@sammamish.us>; Snowanh <snowanh@aol.com>

Subject: Re: 1139 E LAKE SAMMAMISH PARKWAY NE

Thank you: Who in the county has the authority to make decisions and can meet on site?

On Mon, Feb 6, 2017 at 9:40 AM, Lindsey Ozbolt <LOzbolt@sammamish.us> wrote:

Mr. Rohrbach,

Unfortunately, at this time, staff is unable to meet with individual property owners on-site. It is my understanding that King County staff may be able to meet on-site with individual property owners if requested. If you would like to schedule a time to meet with City Staff at City Hall, I am happy to set up a time with you. Additionally, although the official comment period has closed, if you have additional comments you would like to provide, you may submit them to City Hall for consideration.

Best,

Lindsey Ozbolt

Associate Planner | City of Sammamish | Department of Community Development
[425.295.0527](tel:425.295.0527) <tel:(425)%20295-0527>?

Exhibit 29
SSDP2016-00415
002480

From: john rohrbach <upperstarmeadow@gmail.com>
Sent: Monday, February 6, 2017 7:55 AM
To: Lindsey Ozbolt
Subject: Re: 1139 E LAKE SAMMAMISH PARKWAY NE

I need the planner of the trail to meet me at my house to discuss. Please set up. John [206 200 8911](tel:2062008911)

On Fri, Feb 3, 2017 at 4:09 PM, Lindsey Ozbolt
<LOzbolt@sammamish.us<mailto:LOzbolt@sammamish.us>> wrote:
Dear John,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
[425.295.0527](tel:425.295.0527)<[tel:\(425\)%20295-0527](tel:(425)%20295-0527)>

From: john rohrbach [mailto:upperstarmeadow@gmail.com<mailto:upperstarmeadow@gmail.com>]
Sent: Friday, January 27, 2017 10:14 AM
To: Lindsey Ozbolt <LOzbolt@sammamish.us<mailto:LOzbolt@sammamish.us>>>; Snowanh
<snowanh@aol.com<mailto:snowanh@aol.com>>>; john rohrbach
<upperstarmeadow@gmail.com<mailto:upperstarmeadow@gmail.com>>
Subject: 1139 E LAKE SAMMAMISH PARKWAY NE

LINDSEY: We talked yesterday and here are my comments on the trail: We built our house 4 to 5 years ago and our landscape plans as far as the hardscape is concerned were approved by the county. With the current county proposal the county will take our hardscape retaining wall East of our house. The county proposes building their own retaining wall. A lot of money could be saved by not building a retaining wall, since the trail is already very wide there and fairly level and I have an existing wall. I believe the map shows number 38. The map is 69 of 135. There are springs where the county wants to build the wall and it would be a big drainage issue if they were disturbed. The cedar type trees only in front of the house could be retained instead of being removed. These trees only screen the house and not the lake. We need to meet with the trail designers on site and stake and measure everything. A lot of money and aggravation could be saved if we all act intelligently. Please let me know if you receive this e mail. Thank you for your help John and Anh Rohrbach. [206 200 8911](tel:2062008911)<[tel:\(206\)%20200-8911](tel:(206)%20200-8911)>

Exhibit 29
SSDP2016-00415
002481

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Friday, January 27, 2017 4:18 PM
To: 'keithly@mindsring.com'
Subject: RE: Please Approve the Permit for Segment 2B of the ELST

Dear Mark,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

-----Original Message-----

From: Mark Keithly [mailto:keithly@mindsring.com]
Sent: Friday, January 27, 2017 8:11 AM
To: Lindsey Ozbolt <LOzbolt@sammamish.us>
Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the trail permit, as submitted, so that users of all ages and abilities can safely use the trail. A trail built to national standards (AASHTO), that is 12 ft, plus 2 ft gravel shoulders, will allow for safe use by a variety of different users, including people who walk and bike.

As proposed in the permit, priority at trail crossings should be given to the trail and trail users. Consistent crossing priority is intuitive and safe for users of both the trail and the driveways and roads that cross the trail.

When complete, the trail will be an even greater community amenity, and provide a safe option for people who bike to travel to and through Sammamish. Please complete the trail.

I regularly bike ride on the current trail section, and I feel much safer on the trail. I am an avid bike rider and I am looking forward to the trail's completion, so walkers, runners, and bikers can all enjoy the trail in safety..

Sincerely,

Exhibit 29
SSDP2016-00415
002482

Mark Keithly
Kirkland, WA

Mark Keithly
13029 111TH PL NE
Kirkland, WA 98034
(425) 602-5110

Lindsey Ozbolt

From: Microsoft Outlook
<MicrosoftExchange329e71ec88ae4615bbc36ab6ce41109e@sammamish.onmicrosoft.com>
To: keithly@mindsring.com
Sent: Sunday, January 29, 2017 3:53 PM
Subject: Undeliverable: RE: Please Approve the Permit for Segment 2B of the ELST

Delivery has failed to these recipients or groups:

keithly@mindsring.com (keithly@mindsring.com)

Your message couldn't be delivered. Despite repeated attempts to contact the recipient's email system it didn't respond.

Contact the recipient by some other means (by phone, for example) and ask them to tell their email admin that it appears that their email system isn't accepting connection requests from your email system. Give them the error details shown below. It's likely that the recipient's email admin is the only one who can fix this problem.

For more information and tips to fix this issue see this article:
<http://go.microsoft.com/fwlink/?LinkId=389361>.

Diagnostic information for administrators:

Generating server: BY1PR09MB0792.namprd09.prod.outlook.com
Receiving server: BY1PR09MB0792.namprd09.prod.outlook.com
Total retry attempts: 53

keithly@mindsring.com
1/29/2017 11:53:20 PM - Server at BY1PR09MB0792.namprd09.prod.outlook.com returned '550 5.4.300 Message expired'

Original message headers:

DKIM-Signature: v=1; a=rsa-sha256; c=relaxed/relaxed;
d=sammamish.onmicrosoft.com; s=selector1-sammamish-us;
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BY1PR09MB0792.namprd09.prod.outlook.com (10.162.143.22) with Microsoft SMTP
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Exhibit 29
SSDP2016-00415
002484

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Authentication-Results: spf=pass (sender IP is 146.129.253.110)
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id 15.00.1178.000; Fri, 27 Jan 2017 16:17:39 -0800
From: Lindsey Ozbolt <LOzbolt@sammamish.us>
To: "keithly@mindsring.com" <keithly@mindsring.com>
Subject: RE: Please Approve the Permit for Segment 2B of the ELST
Thread-Topic: Please Approve the Permit for Segment 2B of the ELST
Thread-Index: AQHSeLfvTQCpL99X2UuKLIIVTF7CpqFNBl8Q
Date: Sat, 28 Jan 2017 00:17:38 +0000
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References: <1636612366.9220.1485533446090.JavaMail.tomcat@vweb47>
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Exhibit 29
SSDP2016-00415
002485

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Exhibit 29
SSDP2016-00415
002486

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Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Monday, February 6, 2017 10:49 AM
To: Mike Rundle
Subject: Re: Comments on King County SSDP Application

Dear Mike,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: Mike Rundle <mike@rundle.org>
Sent: Friday, January 27, 2017 4:56 PM
To: Lindsey Ozbolt
Cc: Mike Rundle
Subject: FW: Comments on King County SSDP Application

Lindsey,

Attaching a screen shot showing that technical reports are unavailable at end of comment period.

Thank you,

Mike Rundle
425.466.3584

From: Mike Rundle
Sent: Friday, January 27, 2017 4:50 PM
To: 'lozbolt@sammamish.us' <lozbolt@sammamish.us>
Cc: Mike Rundle (mike@rundle.org) <mike@rundle.org>
Subject: Comments on King County SSDP Application

Lindsey,

Please find my comments and exhibits attached.

Additionally, the comment period should be extended an additional 30 days from the date that King County makes the technical reports available for review. They still were not available at the time when I emailed this to you.

Exhibit 29
SSDP2016-00415
002488

Thank you,

Mike Rundle
425.466.3584



King County

Department of Natural Resources and Parks
Division of Parks and Recreation
201 South Jackson Street
Seattle, WA 98104-3855
206-477-477-9378

July 18, 2016

Mr. Ryan Harriman
City of Sammamish, Dept. of Community Development
801 228th Ave SE
Sammamish, WA 98075

SUBJECT: SVAR2016-00155 - Jackman Zoning Variance

Mr. Harriman:

This letter intends to provide comment on the subject notice of application and is submitted on behalf of King County Department of Natural Resources and Parks. King County owns the adjoining property to the east, which is improved with an interim surface regional trail.

After speaking with other King County staff members about this variance, it was determined that the subject site is immediately adjacent to King County property that will be used and redeveloped in the near term, as a paved regional trail. The cross section for the new paved trail will be wider than the current cross section and will expand towards the west boundary of the trail corridor. In particular, the footprint of the paved regional trail will expand 8' – 10' to the west of the current trail and will effectively close off the driveway to the subject undeveloped property (PCN 0724069123), as well as the two undeveloped properties to the north (PCN 0724069124 and 0724069126).

King County Parks anticipates constructing the paved regional trail in approximately 5 years, or less, after receiving the required permits. The trail footprint is expanding to the west to avoid delineated wetlands on the east side of the trail (see enclosed info). Therefore, we note that the access driveway will need to be relocated onto the eastern portion of these private lots. Appropriate consideration for vehicle access and construction activity should be incorporated into the development plans and permits for these 3 lots west of the trail.

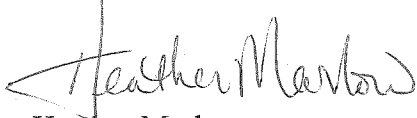
ZONV2016-00155 proposes to reduce the building setback to 0' where 10' is required. A 10' setback from the common property line: 1) provides for private construction and maintenance activities to be performed on private property, rather than from the King County right of way; 2) will accommodate common exterior features such as eaves, gutters, egress windows, vents, light fixtures, doors, front steps,

Exhibit 29
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retaining walls, awnings, landscaping, finish/trim, etc.; and 3) will maintain existing and future private access. **Therefore, King County requests that ZONV2016-00155 be denied.**

Please contact me for additional information or discussion, heather.marlow@kingcounty.gov.

Sincerely,

A handwritten signature in cursive script that reads "Heather Marlow". The signature is written in black ink and is positioned above the typed name.

Heather Marlow
Real Property Agent

Enclosure: Field Notes Wetland 15C
2007 Wetland Rating Form
Wetland Determination
Wetland Tracking Spreadsheet

Lindsey Ozbolt
Associate Planner
East Lake Sammamish Trail
City of Sammamish

Comments regarding King County Permit Application for East Lake Sammamish Trail

The East Lake Sammamish Trail, if done right, will be an amenity to its immediate neighbors and the general public.

Unfortunately King County has again fallen short in their proposed 60% design. It does not accomplish the goal of safety and of being a good neighbor.

Over the last 20 years I have attended every trail planning workshop or meeting I was aware of. I even served as a member of the Citizen's Advisory Group for the East Lake Sammamish Trail. I have submitted written comments, spoke and provided comments and they all seem to fall on deaf ears. The last workshop on this section was several years ago and while they had engineers staffing the meeting they would not discuss specifics because the plan was not ready for review.

This comment period is the first chance to do so in a meaningful way. Thank you for the opportunity to finally comment on specific design issues.

The fact that this is the last segment to permit is not in any way surprising as this is the most contentious area because portions of the trail cut through the middle of Sammamish residential properties. Because of the location of the trail the design issues of safety and privacy are crucial.

The City of Sammamish has a responsibility to its citizens to ensure a safe trail is built that is a good neighbor. Now is the opportunity to get the design right before granting any more permits. Please deny permits until the County addresses safety and design issues created by the proposed widening and changing of the alignment of the trail. Require King County to meet constructively with stakeholders and solve real problems they propose to create so they can come back to the table and reflect solutions in a better design.

Comments Specific to my location:

Tax Parcels 0724069123, 0724069124, 0724069125

Existing Conditions Plan EX5

Plan and Profile AL7 and AL8

Alignment affecting Crossing for Ingress/Egress and Utilities

I have an existing driveway crossing that serves the waterfront lots that was constructed by the railroad over 50 years ago, well before the trail was a twinkle in King County's eyes. In 1999 I bought the property specifically because it had a railroad crossing for ingress/egress and utilities to the waterfront.

Exhibit 29
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The driveway serves 3 waterfront lots zoned for single family residential. While all 3 lots could be built on I chose not to build on one of these lots and it serves as shared waterfront for the eight neighboring houses.

The existing crossing has a special use permit for ingress/egress, utilities, and gates. I was told by King County that the gate to the waterfront is the largest gate installed by King County on the trail. Therefore King County is clearly aware of the importance and necessity of preserving this crossing as they did in the interim trail construction – and the railroad did since the day it was constructed over 50 years ago. (See IMAP picture of existing crossing, gates and fencing).

To ensure we are planning home construction in concert with the trail design I tried to contact the county regarding design for over a year and all I could reach was the agent responsible for crossing permits. He told me that I already had what I needed for special use permits.

Since last year we finally began to be able to get in touch with County Staff. The result being that the County sent the attached letter suggesting that they would move the trail off the centerline and towards the lake “effectively close off the driveway” to all 3 waterfront properties (See attached letter from Heather Marlow - King County).

Clearly this is unacceptable.

The proposed 60% plan reflects this misguided alignment change.

Obviously a trail design that does not preserve our crossing serving 8 homeowners and/or makes that crossing less safe is an improper proposal that needs redesign.

We have subsequently met with King County staff on numerous occasions to understand why they would make such a proposal when they could widen the trail east and maintain and even improve our crossing. Their only reason they claim is they are forced to move away from several small wetlands formed on the east side of the railed created by the elevation of the outflow from the broken culverts (see below). We have looked at this and the trail can protect these marginal wetlands. King County has options to mitigate, buffer average etc. and there is no reason to compromise human safety vs. wetland buffers for marginal wetlands.

They can keep the trail closer to the center of the existing 100+ year old rail alignment and move it eastward to avoid veering towards the lake as demonstrated in the Sorensen Architecture exhibit (see attached).

Keeping closer to the original center line provides several advantages:

- Safer Crossing design
- Trail remains further from Lake Sammamish at present location
- Trail is even further away from Lake Sammamish if moved Eastward
- Less structural walls needed
- Less cost for construction
- Preserve privacy of adjoining neighbors

Drainage

There is a lot of impervious surface being added in the proposal to widen and pave the East Lake Sammamish Trail. We offered to work with the County on trail drainage by allowing them to utilize our storm drain to the lake - they have included this in their design, while ignoring the rest of our input on alignment and safety at the crossing.

Broken Culverts under Rail bed

The proposal does not seem to address the broken culverts for water flowing towards the lake labeled on plan as Unnamed Stream #4 and Unnamed Stream #5. King County dredges stream #5 frequently with a track hoe, so they treat it like a ditch. Stream #4 is a ditch. While the flows are not presently sufficient to support fish, putting a bottomless culvert at a lower elevation at the crossing for Unnamed Stream Crossing #5 (and Unnamed Stream #4) would help provide passage for wildlife. Perhaps a better option would be to divert Unnamed Stream #4 to join Unnamed Stream #5 east of the railbed at one new bottomless culvert at a lower elevation to address the erosion that currently exists while combining crossings and creating greater flows in one path to the lake. There is an opportunity to improve biological functions, so why not do it?

Respectfully,

Robert M. Rundle

2623 E Lake Sammamish Pkwy SE

Sammamish, WA 98075

Location: SE 33rd St to Inglewood Hill Rd. - approx 3.6 miles

Construction Update: This segment is currently in design.

See below for links to construction plans, newsletters, reports, guides, and other project documents.

Recent Activity

60% Design Plans are now available below.

Please note: All comments on the SSDP permit should be sent to the City of Sammamish. Comments can be directed to:

Lindsey Ozbolt, Associate Planner

Phone: [425-295-0527](tel:425-295-0527)

Email: lozbolt@sammamish.us

Mail: City of Sammamish City Hall, 801 228th Avenue SE, Sammamish, Washington 98075.

As the permit applicant, King County Parks staff will attempt to provide information and answers on the trail project, but the City will be collecting and documenting all comments on this permit application. After the close of the comment period, the City of Sammamish will provide all of public comments to King County Parks for our review and consideration.

Need clarification on on the 60% Design Plans? [Schedule a 30 min session](#) on Tuesdays or Wednesdays from January 10-25 to speak with a King County Parks staff member at Sammamish City Hall.

King County Parks will also offer unscheduled drop-in time on Thursdays from 11 am to 3:30 pm on January 12, 19, and 26.

Have questions about upcoming design and construction activities? [Watch these videos](#) to learn more about planting in the trail corridor, sight triangles, trail staking and more.

Design:

[60% Design Plans \(Dec 2016\)](#)

[Tree Preservation Plans \(Jan 2017\)](#)

Guides: Review general design guides about the project here -

- [Sight Distance Triangle Memorandum \(March 2014\)](#)
- [Stop Sign Usage \(July 2013\)](#)
- [Vegetation Management Plan Update \(July 2014\)](#)
- [Readers Guide to Understanding the Design Plans \(April 2014\)](#)
- [How the trail alignment is developed \(Jan 2015\)](#)
- [Landscaping the trail corridor \(Jan. 2015\)](#)
- [Understanding Retaining Walls \(July 2014\)](#)

Exhibit 29
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Technical Reports: ^{SB 16} Check back here to review all technical reports for this segment once available.

Public Outreach:



0724069004

0724069123

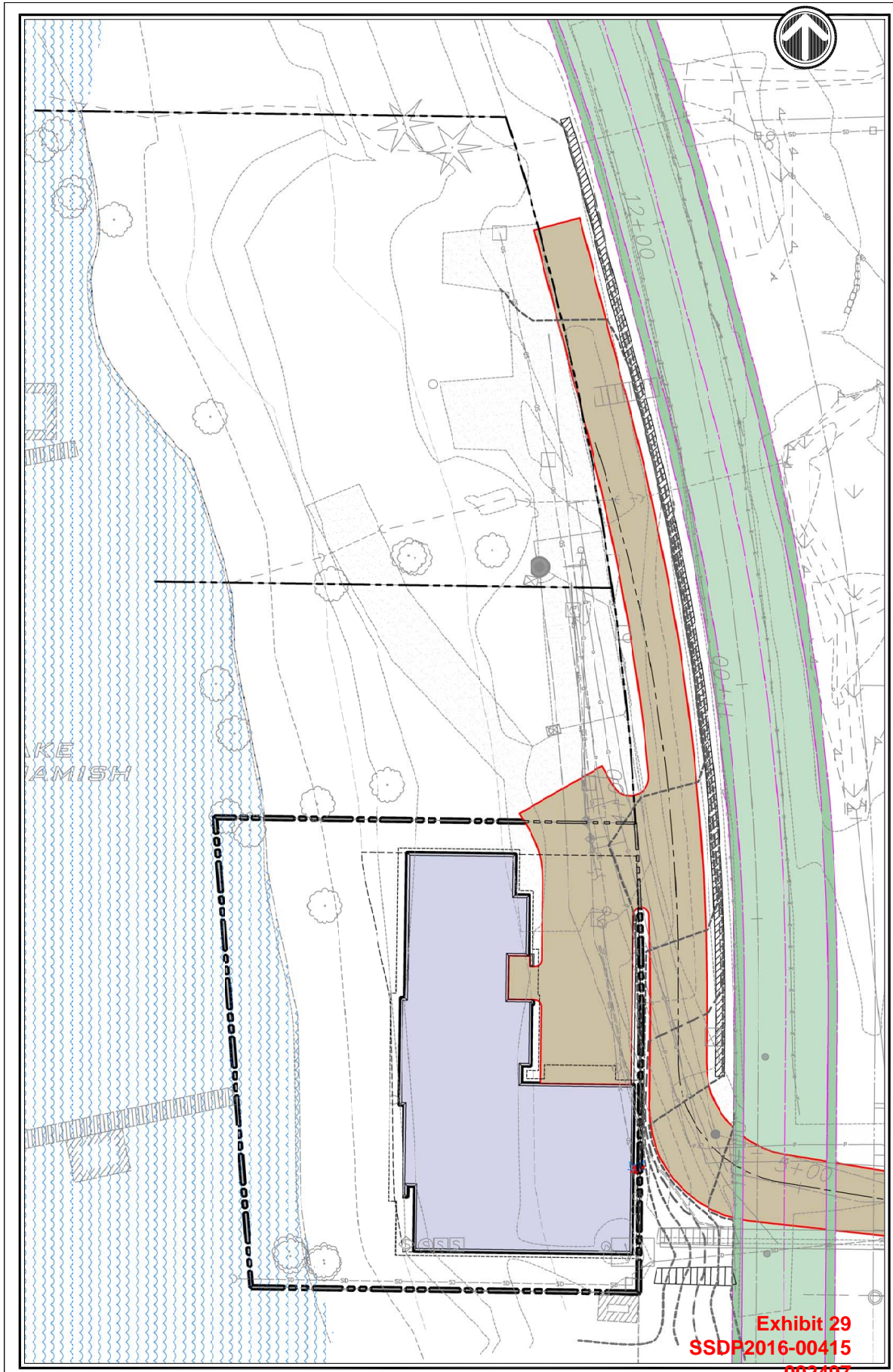
SB-46

Exhibit 29
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002495

DATE	ISSUE/REVISIONS

CONTENTS:

DRAWING NUMBER:
A1.0



S I T E P L A N
 Scale: 1" = 20'-0"

Exhibit 29
SSDP2016-00415
002497

0 1 2 3 4 5 6 7 8 9 10 12 14 16 18 20 22 24 26 28 30 32 34 36 38 40 42 44 46 48 50
 IF THIS DRAWING HAS BEEN REDUCED TO LESS THAN 24" X 36", THEN USE THIS SCALE FOR MEASURING DRAWING

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Friday, January 27, 2017 4:22 PM
To: 'Rwl@gmail.com'
Subject: RE: Please Approve the Permit for Segment 2B of the ELST

Dear Ron,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

-----Original Message-----

From: Ron Lindsay [mailto:Rwl@gmail.com]
Sent: Friday, January 27, 2017 8:38 AM
To: Lindsey Ozbolt <LOzbolt@sammamish.us>
Subject: Please Approve the Permit for Segment 2B of the ELST

Dear

Dear city of Sammamish,

I'm writing to express my support for completing the ELST and approving permit SSDP2016-00415.

Please approve the permit, as submitted.

We ride this trail with our kids. Sometimes we ride out to Isaquah, get a snack, and bus home.

Please approve a standard wide trail and priority right of way for the numerous trail users,

Sincerely,

Ron Lindsay
Mary Ave NW
Seattle, WA 98117
2067785674

Lindsey Ozbolt

From: Microsoft Outlook
<MicrosoftExchange329e71ec88ae4615bbc36ab6ce41109e@sammamish.onmicrosoft.com>
To: Rwl@gmail.com
Sent: Friday, January 27, 2017 4:22 PM
Subject: Undeliverable: RE: Please Approve the Permit for Segment 2B of the ELST



Your message to Rwl@gmail.com couldn't be delivered.

gmail.com suspects your message is spam and rejected it.

LOzbolt

Office 365

gmail.com

Sender

Action Required

Messages suspected as spam

How to Fix It

Try to modify your message, or change how you're sending the message, using the guidance in this article: [Bulk E-mailing Best Practices for Senders Using Forefront Online Protection for Exchange](#). Then resend your message.

If you continue to experience the problem, contact the recipient by some other means (by phone, for example) and ask them to ask their email admin to add your email address, or your domain name, to their allowed senders list.

Was this helpful? [Send feedback to Microsoft](#).

More Info for Email Admins

Status code: 550 5.7.350

When Office 365 tried to send the message to the recipient (outside Office 365), the recipient's email server (or email filtering service) suspected the sender's message is spam.

If the sender can't fix the problem by modifying their message, contact the recipient's

email admin and ask them to add your domain name, or the sender's email address, to their list of allowed senders.

Although the sender may be able to alter the message contents to fix this issue, it's likely that only the recipient's email admin can fix this problem. Unfortunately, Office 365 Support is unlikely to be able to help fix these kinds of externally reported errors.

Original Message Details

Created Date: 1/28/2017 12:22:06 AM
Sender Address: LOzbolt@sammamish.us
Recipient Address: Rwl@gmail.com
Subject: RE: Please Approve the Permit for Segment 2B of the ELST

Error Details

Reported error: *550 5.7.350 Remote server returned message detected as spam -> 550 permanent failure for one or more recipients (rwl@gmail.com:550 5.1.1 The email account that you tried to reach does not exist. Please try 5.1.1 double-checking th...)*
DSN generated by: DM5PR09MB1196.namprd09.prod.outlook.com
Remote server: mx2.ess.sjf.cudaops.com

Message Hops

HOP	TIME (UTC)	FROM	TO	WITH
1	1/28/2017 12:22:07 AM	CHMAIL001.cityofsammamish.local	CHMail001.cityofsammamish.local	mapi
2	1/28/2017 12:22:07 AM	CHMAIL001.cityofsammamish.local	CHMail001.cityofsammamish.local	Microsoft SMTP Server
3	1/28/2017 12:22:07 AM	CHMail001.cityofsammamish.local	BY2NAM03FT034.mail.protection.outlook.com	Microsoft SMTP Server cipher=TLS_ECDHE_RSA
4	1/28/2017 12:22:08 AM	BY2NAM03FT034.eop- NAM03.prod.protection.outlook.com	BLUPR09CA0020.outlook.office365.com	Microsoft SMTP Server cipher=TLS_ECDHE_RSA
5	1/28/2017 12:22:08 AM	BLUPR09CA0020.namprd09.prod.outlook.com	DM5PR09MB1196.namprd09.prod.outlook.com	Microsoft SMTP Server cipher=TLS_ECDHE_RSA

Original Message Headers

DKIM-Signature: v=1; a=rsa-sha256; c=relaxed/relaxed;
d=sammamish.onmicrosoft.com; s=selector1-sammamish-us;
h=From:Date:Subject:Message-ID:Content-Type:MIME-Version;
bh=7hIQMOB0WyOBzZGaof4DEC0sF5ecptqe26w6K35xGes=;

Exhibit 29
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002500

b=XQvyr38Y8XnFq2BTmUlpT36PdYVnLxEmqhhbivwJP8glj9aHhHK0jXwO4wzoCC/rZluzfv/oMsAupIc3aCpuG3H
RAKvRX5zTg1AYR7F4hSPRqFZpIvt/lPpj2Gz3DPjAlA0k50tFodLCTCQR2CKKGU1OGnk3R5N/osg/JPZROR8=
Received: from BLUPR09CA0020.namprd09.prod.outlook.com (10.255.214.148) by
DM5PR09MB1196.namprd09.prod.outlook.com (10.172.33.146) with Microsoft SMTP
Server (version=TLS1_2, cipher=TLS_ECDHE_RSA_WITH_AES_256_CBC_SHA384_P384) id
15.1.860.13; Sat, 28 Jan 2017 00:22:08 +0000
Received: from BY2NAM03FT034.eop-NAM03.prod.protection.outlook.com
(2a01:111:f400:7e4a::207) by BLUPR09CA0020.outlook.office365.com
(2a01:111:e400:8b7::20) with Microsoft SMTP Server (version=TLS1_2,
cipher=TLS_ECDHE_RSA_WITH_AES_256_CBC_SHA384_P384) id 15.1.874.12 via
Frontend Transport; Sat, 28 Jan 2017 00:22:08 +0000
Authentication-Results: spf=pass (sender IP is 146.129.253.110)
smtp.mailfrom=sammamish.us; gmail.com; dkim=none (message not signed)
header.d=none;gmail.com; dmarc=bestguesspass action=none
header.from=sammamish.us;
Received-SPF: Pass (protection.outlook.com: domain of sammamish.us designates
146.129.253.110 as permitted sender) receiver=protection.outlook.com;
client-ip=146.129.253.110; helo=CHMail001.cityofsammamish.local;
Received: from CHMail001.cityofsammamish.local (146.129.253.110) by
BY2NAM03FT034.mail.protection.outlook.com (10.152.84.211) with Microsoft SMTP
Server (version=TLS1_2, cipher=TLS_ECDHE_RSA_WITH_AES_256_CBC_SHA384_P384) id
15.1.874.2 via Frontend Transport; Sat, 28 Jan 2017 00:22:07 +0000
Received: from CHMAIL001.cityofsammamish.local (10.1.1.15) by
CHMail001.cityofsammamish.local (10.1.1.15) with Microsoft SMTP Server (TLS)
id 15.0.1178.4; Fri, 27 Jan 2017 16:22:07 -0800
Received: from CHMAIL001.cityofsammamish.local ([fe80::a4f2:1e99:c121:b116])
by CHMail001.cityofsammamish.local ([fe80::a4f2:1e99:c121:b116%12]) with mapi
id 15.00.1178.000; Fri, 27 Jan 2017 16:22:07 -0800
From: Lindsey Ozbolt <LOzbolt@sammamish.us>
To: "Rwl@gmail.com" <Rwl@gmail.com>
Subject: RE: Please Approve the Permit for Segment 2B of the ELST
Thread-Topic: Please Approve the Permit for Segment 2B of the ELST
Thread-Index: AQHSeLvB5XH6Js3eEEasJjvO13pCV6FNB5Uw
Date: Sat, 28 Jan 2017 00:22:06 +0000
Message-ID: <489757e898e44ec1b1221c478c5e944f@CHMail001.cityofsammamish.local>
References: <2105237363.9131.1485535090287.JavaMail.tomcat@vweb65>
In-Reply-To: <2105237363.9131.1485535090287.JavaMail.tomcat@vweb65>
Accept-Language: en-US
Content-Language: en-US
X-MS-Has-Attach:
X-MS-TNEF-Correlator:
x-ms-exchange-transport-fromentityheader: Hosted
x-originating-ip: [10.1.1.155]
Content-Type: text/plain; charset="utf-8"
Content-Transfer-Encoding: base64
MIME-Version: 1.0

Return-Path: LOzbolt@sammamish.us

X-EOPAttributedMessage: 0

X-Forefront-Antispam-Report:

CIP:146.129.253.110;IPV:NLI;CTRY:US;EFV:NLI;SFV:NSPM;SFS:(10019020)(6009001)(7916002)(39450400003)(2980300002)(438002)(199003)(13464003)(189002)(377454003)(1411001)(107886002)(39060400001)(38730400001)(5250100002)(305945005)(2906002)(50466002)(189998001)(108616004)(6916009)(7696004)(23676002)(80792005)(356003)(2950100002)(450100001)(33646002)(110136003)(47776003)(24736003)(106116001)(6116002)(98436002)(5640700003)(3846002)(7736002)(102836003)(86362001)(74482002)(229853002)(2351001)(5660300001)(106466001)(626004)(81156014)(81166006)(8676002)(9686003)(50986999)(76176999)(345774005)(2900100001)(54356999)(2501003)(8936002)(92566002)(561944003)(80162004)(80862006);DIR:OUT;SFP:1102;SCL:1;SRVR:DM5PR09MB1196;H:CHMail001.cityofsammamish.local;FPR:;SPF:Pass;PTR:mail.sammamish.us;A:1;MX:1;LANG:en;

X-Microsoft-Exchange-Diagnostics:

1;BY2NAM03FT034;1:shZfGm6sF7UcJkUscFAdms7jNWCioxSwJ+jd8ZLF0hk3oAFdyRcp7D9Frwlq8rRKoCTX9DPY9bY1PiRppBuF657HU56Zy6b7gVNSDPAaPttPr2PC9MOSAlWBjNk7b1IfDav4gD8Km6NRKxjd5q2jQW2LcrvqDDM5WluRNuiTylHIW2nuymbdjn6jO2Se71JHWcwQqFDfZv5Vp4wBoLLeb089otjJAsoltNV1Dv39UOJYduaCQZA7dzfusdEMpkipRmlm9vfiw34Xi3MzMoNAFMkirEplx2U9g8wLpMUja8KJmfpc3w9yptQgADW2ms1AmTnVuPZmKnlHaZEhIX/kU7sL3OPQhejAN2Nb8XDUqlacHS/bizLjNd998gN/qdYKX/vH/frJvJjSRa6zq+JSkA4NCqC2e/zFgcphE7H69TS/PaujylgLfiah2GVjBLwGVSQE3UCdJIudu9xlyJc60OgPFP2Fz8mqEkiDpGYd/PU2wRnLkIdmDg/gT4OrQ5QF066ogotldiVxHrfookaKkGoA==

X-MS-Office365-Filtering-Correlation-Id: 6ecd6e55-7d19-4b9b-ef5b-08d44713b1d9

X-Microsoft-Antispam:

UriScan:;BCL:0;PCL:0;RULEID:(22001)(8251501002);SRVR:DM5PR09MB1196;

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1;DM5PR09MB1196;3:86mX6PiYirbZTVvsCUxKqbt+GB8ptRERGTddVyawk1WYqT5Iq7Nht53Hqkvz0ko3goP/YkXZULGA0y0qPybYVuuQ3iMFu2XTta8LSafuGSCqDvRoS3z214p/C0ORNPkryD2GZuYJKC5vH7oK7EJ3Q7N2SXuaDF2s6U//iHdJktmiyfs/utTZC9gRUIV4o72V8GXbHksL90g92VI7YqLFILO2PD7o028Fn9ns3T78b9Kks1o/8EOrrpckLOQ5IMfX4nqsp6ABXAWJ0U5THMO9i0JjbykrZedVV8AEGNOaFJUd3gsVuzKzGYG8Eu8KFWOWP/KnVex/pm13rfrdKqJqq/TRmcGeh4dilaFuiZuQA0kKej62VarZNPyp/EX0m2dGfSflisWZ+C64voG4SbC4cA==

X-Microsoft-Exchange-Diagnostics:

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X-Microsoft-Exchange-Diagnostics:

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X-Microsoft-Antispam-PRVS:

<DM5PR09MB11962106AFBED50594CDF4AFD2490@DM5PR09MB1196.namprd09.prod.outlook.com>

X-Exchange-Antispam-Report-Test: UriScan:;

X-Exchange-Antispam-Report-CFA-Test:

BCL:0;PCL:0;RULEID:(6040375)(601004)(2401047)(8121501046)(5005006)(13015025)(13017025)(13023025)(13024025)(13018025)(3002001)(10201501046)(6041248)(20161123555025)(20161123564025)(20161123562025)(20161123560025)(6072148);SRVR:DM5PR09MB1196;BCL:0;PCL:0;RULEID:;SRVR:DM5PR09MB1196;

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002502

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X-Forefront-PRVS: 02015246A9

X-Microsoft-Exchange-Diagnostics:

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X-Microsoft-Exchange-Diagnostics:

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SpamDiagnosticOutput: 1:99

SpamDiagnosticMetadata: NSPM

X-Microsoft-Exchange-Diagnostics:

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X-OriginatorOrg: sammamish.us

X-MS-Exchange-CrossTenant-OriginalArrivalTime: 28 Jan 2017 00:22:07.8789

(UTC)

X-MS-Exchange-CrossTenant-Id: 6e7447e6-2908-43ac-b198-8fe679e1a51d

X-MS-Exchange-CrossTenant-OriginalAttributedTenantConnectingIp: TenantId=6e7447e6-2908-
43ac-b198-8fe679e1a51d;Ip=[146.129.253.110];Helo=[CHMail001.cityofsammamish.local]

X-MS-Exchange-CrossTenant-FromEntityHeader: HybridOnPrem

X-MS-Exchange-Transport-CrossTenantHeadersStamped: DM5PR09MB1196

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Monday, February 6, 2017 10:15 AM
To: erniem@marchandnorthwest.com
Subject: Re: East Lake Sammamish Trail

Dear Ernie,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: erniem@marchandnorthwest.com <erniem@marchandnorthwest.com>
Sent: Friday, January 27, 2017 4:20 PM
To: Lindsey Ozbolt
Subject: East Lake Sammamish Trail

Lindsey Ozbolt
2017
Associate Planner
City of Sammamish

January 27,

RE: East Lake Sammamish Master Plan Trail

Dear Lindsey,

I am writing on behalf of my family regarding the East Lake Sammamish Master Plan Trail.

Members of my family and I own two recreational lots, number 36 and 37. I'm not sure of the Block number, however, we are adjacent to the Inglewood Hills Beach lots, on the North side of IBC. Our lots are used on a regular basis, year round, by our families and regularly by our guests.

We have been very supportive of the Trail and we are eager to see its completion.

That said, I understand that we will be losing our gate and stairs down to the property and that we will have to access through an opening and shared stairs, passing through the ROW and by several of our neighbor's properties.

This concerns us for a number of reasons, including safety, security, liability and for a host of other, potential practical reasons.

I think you will agree that not having independent access is detrimental to the value of our property. Furthermore, a stairway without a gate is an open invitation for vandals, unauthorized partying by minors, garbage, etc., not to mention other acts youth under the influence tend to do at night.

I'm sure you would agree that these concerns are legitimate and regardless of Law Enforcement Patrols, they will occur and we, as the property owners, and tax payers, will suffer the consequences.

Regarding underage youth having unhindered access to the beach and their inevitable drinking (alcohol) and marijuana/drug use, and the number of unintended consequences, the question must be asked, who is liable if the County provides open access our beach from the Trail?

What are the consequences if someone hurts themselves, or injures another, cause a fire, or dies for some unforeseen reason?

Obviously, if the County removes the existing fence and gates, the County would be.

Please note that one of the most significant benefits of the existing trail and its fence and gate installation is that even with the increased foot and bike traffic, underage drinking, trash, vandalism, open fires, etc., has been reduced significantly BECAUSE OF THE LOCKED GATES.

We ask that you please reconsider the material harm we will suffer if you remove the gate and direct access to our property and that as good neighbors and citizens, I hope that we can get together and discuss a satisfactory, alternative solution?

Thank you!

Best regards, Ernie Marchand – 206-619-7910

Along with Tyler Marchand, Mike, Jana, Robbie and Courtney Marchand, Albert Delgado, Patty and Laura Marchand, Alan Marchand

Lindsey Ozbolt
Associate Planner
City of Sammamish

January 27, 2017

RE: East Lake Sammamish Master Plan Trail

Dear Lindsey:

I am writing on behalf of my family regarding the East Lake Sammamish Master Plan Trail.

Members of my family and I own two recreational lots, number 36 and 37. I'm not sure of the Block number, however, we are adjacent to the Inglewood Hills Beach lots, on the North side of IBC.

Our lots are used on a regular basis, year round, by our families and regularly by our guests.

We have been very supportive of the Trail and we are eager to see its completion.

That said, I understand that we will be losing our gate and stairs down to the property and that we will have to access through an opening and shared stairs, passing through the ROW and by several of our neighbor's properties.

This concerns us for a number of reasons, including safety, security, liability and for a host of other, potential practical reasons.

I think you will agree that not having independent access is detrimental to the value of our property. Furthermore, a stairway without a gate is an open invitation for vandals, unauthorized partying by minors, garbage, etc., not to mention other acts youth under the influence tend to do at night.

I'm sure you would agree that these concerns are legitimate and regardless of Law Enforcement Patrols, they will occur and we, as the property owners, and tax payers, will suffer the consequences.

Regarding underage youth having unhindered access to the beach and their inevitable drinking (alcohol) and marijuana/drug use, and the number of unintended consequences, the question must be asked, who is liable if the County provides open access our beach from the Trail?

What are the consequences if someone hurts themselves, or injures another, cause a fire, or dies for some unforeseen reason?

Obviously, if the County removes the existing fence and gates, the County would be.

Please note that one of the most significant benefits of the existing trail and its fence and gate installation is that even with the increased foot and bike traffic, underage drinking, trash, vandalism, open fires, etc., has been reduced significantly BECAUSE OF THE LOCKED GATES.

We ask that you please reconsider the material harm we will suffer if you remove the gate and direct access to our property and that as good neighbors and citizens, I hope that we can get together and discuss a satisfactory, alternative solution?

Thank you!

Best regards, Ernie Marchand – 206-619-7910

Along with Tyler Marchand, Mike, Jana, Robbie and Courtney Marchand, Albert Delgado, Patty and Laura Marchand, Alan Marchand.

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Monday, February 6, 2017 10:13 AM
To: Samuel A. Rodabough
Subject: Re: Hild Public Comment - SSDP2016-00415

Dear Mr. Rodabough,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: Samuel A. Rodabough <sam@rodaboughlaw.com>
Sent: Friday, January 27, 2017 4:14 PM
To: Lindsey Ozbolt; gina.auld@kingcounty.gov
Cc: Flemming, Barbara
Subject: Hild Public Comment - SSDP2016-00415

Ms. Ozbolt and Ms. Auld,

On behalf of my clients Bob & Janet Hild, please see a comment letter attached in pdf format regarding the above shoreline substantial development permit for the East Lake Sammamish Trail, South Sammamish B Segment. Please let me know if you require anything further. I look forward to working with the City and County to resolve my clients' concerns.

Regards,

Samuel A. Rodabough
Law Office of Samuel A. Rodabough PLLC
11820 Northup Way, Ste. E200
Bellevue, WA 98005
(425) 440-2593 (phone)
(425) 284-3051 (fax)
sam@rodaboughlaw.com

NOTICE: This communication may contain privileged or other confidential information. If you have received it in error, please advise the sender by reply email and immediately delete the message and any attachments without copying or disclosing the contents. Thank you.

Exhibit 29
SSDP2016-00415
002509



LAW OFFICE OF
SAMUEL A. RODABOUGH PLLC

SAMUEL A. RODABOUGH
ATTORNEY AT LAW
11820 NORTHUP WAY, STE. E200
BELLEVUE, WA 98004
(425) 440-2593
(425) 284-3051 (FAX)

January 27, 2017

Via Email & Hand Delivery

City of Sammamish
Department of Community Development
Attn: Lindsey Ozbolt, Associate Planner
801 228th Ave. SE
Sammamish WA, 98075
lozbolt@sammamish.us

King County
Department of Natural Resources and Parks
Attn: Gina Auld, Capital Project Manager IV
201 S. Jackson St., Ste. 700
Seattle, WA 98104-3855
gina.auld@kingcounty.gov

**Re: Shoreline Substantial Development Permit 2016-00415
East Lake Sammamish Trail, South Sammamish B Segment
Hild Property, King County Tax Parcel No. 0624069123**

Dear Ms. Ozbolt and Ms. Auld:

This Firm represents Robert & Janet Hild, the owners of a residence located at 1204 East Lake Sammamish Parkway SE, Sammamish, WA 98075. This residence is located on an uphill slope immediately east of East Lake Sammamish Parkway. Relevant for purposes of this letter, my clients also own a separate parcel that is located downslope from their residence and is currently used for recreational purposes. This parcel contains approximately 60 feet of frontage on Lake Sammamish and is improved with a dock, boathouse, and deck, the existence of which predate their purchase in May of 2000. This parcel is known as King County Tax Parcel No. 0624069123 (“Hild Property”).

My clients are in receipt of the City’s Notice of Application for the above SSDP and they have reviewed the 60% design plans for the Trail, dated on or about September 2016 (“Preliminary Plans”). The Hild Property will be adversely affected by the proposed modifications to the East Lake Sammamish Trail (“Trail”) that have been proposed by King County (“County”) in the above shoreline substantial development permit (“SSDP”). Please accept the following as (1) a response on behalf of my clients to the SSDP application, including the Preliminary Plans, and (2) a request for my clients to be included as parties of record for this SSDP and to receive future notifications and status updates regarding the SSDP application.

A. Property Interests

As an initial matter, it is prudent to note that the nature of the property interests involved with respect to the Trail and adjoining properties have been the subject of various, and sometimes even conflicting, adjudications by state and federal courts. It is my clients’ understanding that some of these judicial proceedings are still pending. Accordingly, nothing in this letter is intended to be construed as bearing on the status of those property interests and my clients

**Exhibit 29
SSDP2016-00415
002510**

reserve, to the fullest extent of the law, any property interest that they may have in the area burdened by the Trail.

B. Incomplete Preliminary Plans

It is also necessary to observe that the ability to completely assess the full impact of the Preliminary Plans upon the Hild Property was necessarily limited by incomplete surveying work and/or an omission in the Preliminary Plans. In particular, as indicated above, the Hild Property is improved with a dock, boathouse, and deck. For unknown reasons, however, although all existing improvements on the adjoining parcels to the immediate north and south are depicted in the Preliminary Plans, my clients' boathouse and deck were omitted.¹ A data sheet from the King County Assessor regarding my clients' property, which includes a photo of the deck, is attached hereto.

In the absence of this rudimentary information from a complete and accurate survey, my clients are left to speculate regarding the true impacts of the Preliminary Plans upon their property. However, utilizing the tools available to them, including some aerial photography, the following comments have been prepared based upon the following assumptions, which are subject to change based upon more complete information: (1) all or most of the existing boathouse is located wholly within the boundaries of the Hild Property, (2) all or most of the associated deck is located within the right of way for the Trail.

C. Impacts of Preliminary Plans

A review of the Preliminary Plans indicates that the County's project will have adverse impacts on the Hild Property, including the following:

- **Impairment of Access** – My clients currently access their property via a stairway that commences from East Lake Sammamish Parkway and proceeds downslope to the Trail. This existing stairway is depicted on the Preliminary Plans between stations 373+00 and 374+00.² The Preliminary Plans propose the permanent elimination of this stairway.³ The elimination of this stairway will require my clients to access their property further to the south by entering the Trail at the crossing at near station 371+00 and then backtracking to reach their property.⁴ For obvious reasons, my clients do not support removal of this stairway. Moreover, the County has previously represented that retaining such access points would be a priority in the Trail design.

¹ See Preliminary Plans, Existing Conditions Plan, at pg. EX12 (attached hereto).

² See Preliminary Plans, Existing Conditions Plan, at pg. EX12 (attached hereto).

³ See Preliminary Plans, Plan and Profile, at pg. AL19 (attached hereto).

⁴ See Preliminary Plans, Plan and Profile, at pg. AL19 (attached hereto).

- **Safety/Privacy** – From the Trail, my clients currently access their property via a locked gate depicted on the Preliminary Plans between stations 373+00 and 374+00.⁵ The Preliminary Plans propose widening the Trail, which will result in the construction of a block retaining wall on its west side.⁶ This wall will be exposed approximately 5 feet above the existing grade when viewed from the west.⁷ In order to bridge the elevation difference between the widened Trail and the lower portion of the County’s right of way and the Hild Property, the Preliminary Plans depict the construction of a concrete stairway identified as “Stair #63.”⁸ It appears that this stairway is designed to both facilitate access by the County for maintenance of the new retaining wall and for private access to the Hild Property.

As confirmed by the County, however, these stairs will not contain any gate, let alone a locked gate comparable to my clients’ existing one. This gate has been necessary to maintain the safety of my clients’ valuable boat (and other personal property) and maintain privacy in utilizing their recreational amenities. My clients recognize the necessity for widening the Trail and the accompanying need to construct a new access stairwell. However, they do not support the construction of an unlocked stairway that will facilitate, and perhaps even encourage, access to their property by Trail users. Although the County has suggested that my clients install a privacy and security fence at the east boundary line of the Hild Property, such a fence would be located within just a few feet of ordinary high water, which may not only be undesirable from a permitting standpoint, but may unnecessarily impede visual access to the water.

- **Wetland Mitigation** – The Preliminary Plans identify two alleged wetlands (Wetlands 23A and 23B) and one alleged jurisdictional ditch (Jurisdictional Ditch #14) in the vicinity of the Hild Property.⁹ The limited time available for public comment has not afforded my clients an opportunity to retain a biologist to determine if he or she agrees with the wetland category and rating assigned to each of these wetlands and the alleged jurisdictional nature of the ditch. That being said, inasmuch as wetlands are identified by the presence of soils, hydrology, and vegetation, my clients do not believe that these wetlands meet the appropriate definitions to be regulated as such under local, state, and/or federal law.

Not only are these areas generally lacking in these elements, but to the extent that said wetlands exist, they have been artificially created as a result of modifications to the

⁵ See Preliminary Plans, Existing Conditions Plan, at pg. EX12 (attached hereto).

⁶ See Preliminary Plans, Plan and Profile, at pg. AL19 (attached hereto).

⁷ See Preliminary Plans, Wall Profiles, at pg. WP6 (attached hereto).

⁸ See Preliminary Plans, Plan and Profile, at pg. AL19 (attached hereto).

⁹ See Preliminary Plans, Existing Conditions Plan, at pg. EX12 (attached hereto).

grade of the former rail corridor and current Trail. *See* SMC 21A.15.1415 (“Wetlands do not include those artificial wetlands intentionally created from nonwetland sites, including, but not limited to, irrigation and drainage ditches, grass-lined swales, canals, detention facilities, wastewater treatment facilities, farm ponds, and landscape amenities, or those wetlands created after July 1, 1990, that were unintentionally created as a result of the construction of a road, street, or highway.”). As such, they should not be regulated as wetlands and impacts to those alleged wetlands and/or accompanying buffers should not be required.

Unfortunately, the Preliminary Plans reveal that approximately 1,000 feet (or more) of “wetland buffer addition area” will be planted in the very location where my clients’ longstanding deck is situated.¹⁰ My clients are concerned that this mitigation may require the removal of their longstanding deck. As indicated, however, it does not appear that these wetlands meet the applicable criteria to be designated as such, so no such mitigation should be required. *See* SMC 21A.15.1415. Moreover, it appears that the County is largely exempt from mitigating wetland buffer impacts as a result of Trail. *See* SMC 21A.50.290(2)(a) (“Where...the East Lake Sammamish Trail transects a wetland buffer, the department may approve a modification of the standard buffer width to the edge of...the East Lake Sammamish Trail if the isolated part of the buffer does not provide additional protection of the wetland and provides insignificant biological, geological or hydrological buffer functions relating to the wetland.”). In short, as a result of this provision, the Preliminary Plans should not depict or otherwise project buffers onto the opposite side of the Trail from the respective “wetlands,” let alone mitigate for alleged impacts to their non-existent buffers. In summary, requiring mitigation in the proposed location of my clients’ longstanding deck appears to be wholly unnecessary and an equally unwise use of taxpayer resources.

D. Preferred Resolutions

On January 27, 2017, the undersigned and Mr. Hild attended a productive meeting with County representatives to discuss the potential adverse impacts to the Hild Property as a result of the Preliminary Plans. The County representatives in attendance included Barbara Flemming, Senior Deputy Prosecuting Attorney and Frank Overton, Capital Projects Managing Supervisor for the King County Department of Natural Resources and Parks. My clients were very much appreciative of the tone of the meeting and the County’s willingness to consider creative options for the Hild Property.

Although nothing concrete emerged from this meeting, Mr. Hild expressed a potential willingness to grant the County a covenant or easement that would allow the installation of a storm drainage pipe under and through the Hild Property for a direct discharge into the Lake. In turn, this would save taxpayers the installation of a very expensive infiltration trench depicted on the Preliminary Plans.

¹⁰ *See* Preliminary Plans, Landscape Plan, at pg. LA12 (attached hereto).

Additionally, my clients are also considering the potential removal of their boathouse, which is constructed upon a retaining wall that now presumably represents the ordinary high water mark on that portion of the Hild Property. Removal of this bulkhead and the accompanying boathouse would presumably result in a net increase of shoreline ecological functions and values and allow for more meaningful mitigation than the seemingly arbitrary "wetland buffer addition area" currently depicted in the area containing my clients' longstanding deck. In turn, the County may be willing to grant my clients a special use permit to (1) retain their existing deck, (2) construct a new boathouse on the upland portion of right of way in the vicinity of the new retaining wall, and/or (3) install a locked gate to preserve the safety and privacy of the Hild Property.

In summary, to the extent that wetland mitigation is required in the vicinity of the Hild Property, my clients respectfully request that County and City staff employ some regulatory flexibility, creativity, and patience to determine if the parties can reach a mutually beneficial resolution.

CONCLUSION

My clients do not generally oppose the improvements to the Trail and hope that the County is able to fulfill its vision for the corridor. They openly recognize that the property and permitting issues involved in this segment of the Trail are complex and will necessarily require some time to analyze and resolve. My clients look forward to working with the City and County to successfully resolve their concerns.

Sincerely,

LAW OFFICE OF SAMUEL A. RODABOUGH PLLC

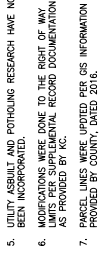
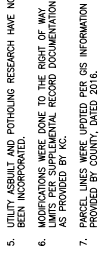


Samuel A. Rodabough

sam@rodaboughlaw.com

cc: Barbara Flemming, Senior Deputy Prosecuting Attorney

- GENERAL NOTES:**
- SEE SHEET 04 FOR SYMBOL AND LINE LEGEND.
 - THE RAIL ROAD CENTRALINE (RR C/L) & R/W LINES PROVIDED BY KING COUNTY, DATED 1994, UNLESS OTHERWISE NOTED.
 - OUTLINES FOR BUILDING STRUCTURES SHOWN ARE BASED ON GS AND AERIAL PHOTOS, DATED 2012.
 - NOT ALL HEEDS AND ABBREVIATIONS ARE INCLUDED IN THE SURVEY.
 - UTILITY ASSESSMENT AND PHOTOLOGIC RESEARCH HAVE NOT BEEN INCORPORATED.
 - MODIFICATIONS WERE DONE TO THE RIGHT OF WAY AS PROVIDED BY KING COUNTY, DATED 1994, UNLESS OTHERWISE NOTED.
 - RAILROAD LINES WERE LOCATED PER OS INFORMATION PROVIDED BY COUNTY, DATED 2016.



60% REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

SHEET NO.	20 OF 135
PROJECT NAME	EAST LAKE SAMMAMISH MASTER PLAN TRAILS SOUTH SAMMAMISH SEGMENT B

EXISTING CONDITIONS PLAN

PROJECT NAME: EAST LAKE SAMMAMISH MASTER PLAN TRAILS SOUTH SAMMAMISH SEGMENT B
SMMAMISH, WA

PARAMETRIX CONSULTING ENGINEERS 7515 2ND AVENUE, SUITE 200 | EAST, WA 98524
P: 360-944-2700 WWW.PARAMETRIX.COM

ONE INCH AT FULL SCALE, IF NOT SCALE ACCORDINGLY
BL1521075819103EX-03
P. JOHANNESSEN
SEP 11 2016 - 075 413 103
APPY BY: PD
SEPTEMBER 2016

REVISIONS	DATE	BY
DESIGNED		M. TSUN
DRAWN		P. PURGAYAN
CHECKED		P. JOHANNESSEN
APP'D BY		PD

EAST LAKE SAMMAMISH PARKWAY SE

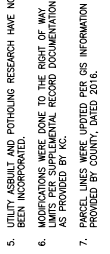


EXHIBIT 09
SDP2016-00415
002510

LAYOUTS EX12
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PLOTTED BY: purgayan.p
DATE: Wednesday, October 12, 2016 8:37:53 PM

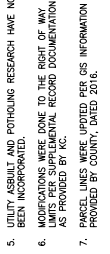
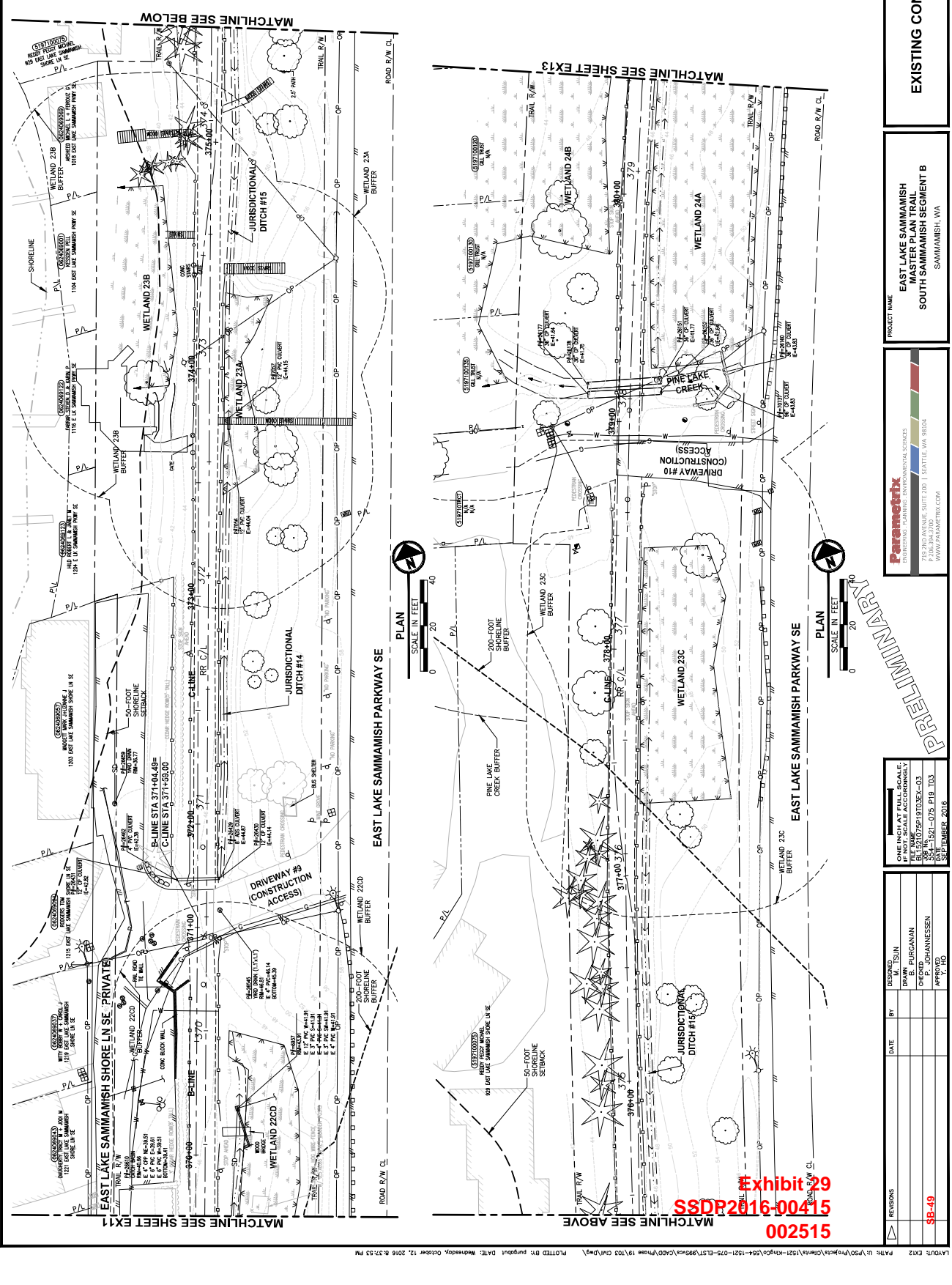


EXHIBIT 09
SDP2016-00415
002510

LAYOUTS EX12
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PLOTTED BY: purgayan.p
DATE: Wednesday, October 12, 2016 8:37:53 PM



PRELIMINARY

MATCHLINE SEE BELOW

MATCHLINE SEE ABOVE

MATCHLINE SEE BELOW

MATCHLINE SEE ABOVE

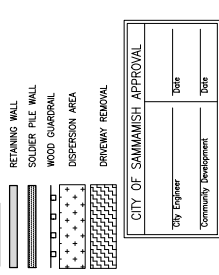
CIVIL CONSTRUCTION NOTES:

- 1 RESTORE AND INSTALL STAR, TYPE TO BE DETERMINED.
- 2 RECONSTRUCT PEDESTRIAN BRIDGE.
- 3 INSTALL PRECAST REINFORCED CONCRETE SPILT BOX CURB.
- 4 SEE TP SHEETS FOR EXISTING WALL. SEE WALL DETAILS ON SHEET X. SEE WALL PROFILES ON WP SHEETS.
- 5 INSTALL COATED CHAIN LINK FENCE TYPE 6, FOR GROUND POST INSTALLATION. SEE APPENDIX B IN CONSTRUCTION MANUAL. SEE SHEET A FOR MOORING ON TOP OF WALL.
- 6 NEW DRIVEWAY.
- 7 INSTALL WOOD GUARDRAIL. SEE DETAILS ON SHEET XL.
- 8 INSTALL SPLIT RAIL FENCE. SEE DETAILS ON SHEET XC.
- 9 INSTALL GRANITE BLOCK WALL. SEE WALL DETAILS ON SHEET WP. SEE WALL PROFILES ON WP SHEETS.
- 10 DRIVEWAY REMOVAL. SEE WP SHEETS FOR PLANTING PLAN.
- 11 INSTALL PATTERED CONCRETE DRIVEWAY. SEE DETAILS ON SHEET XX.
- 12 INSTALL TYPE 1 REST STOP. SEE DETAILS ON SHEET XX.
- 13 INSTALL TYPE 2 REST STOP. SEE DETAILS ON SHEET XX.
- 14 INSTALL SOLID PILE WALL.
- 15 INSTALL GRANITE BLOCK WALL.
- 16 INSTALL MINE WALL. SEE PF SHEETS.
- 17 INSTALL EXTENDED CURB PER MDSOT STD PLAN P-1042-00.
- 18 REST STOP PULL-OUT.
- 19 STARS WILL BE ELIMINATED DURING CONSTRUCTION.
- 20 BRIDGES WILL BE REMOVED TO R/W LINE DURING CONSTRUCTION.
- 21 INSTALL ROCK WALL.

STORMWATER CONSTRUCTION NOTES:

- 1 INFILTRATION TRENCH. SEE DETAIL 4 ON SHEET DD1.
- 2 UNDERDRAIN TRENCH. SEE DETAIL 5 ON SHEET DD1.
- 3 ROCK PAD. SEE DETAIL 2 ON SHEET DD1 AND WP SHEETS.
- 4 INSTALL UNDERDRAIN TO DRAINAGE STRUCTURE AND CLEANING EXISTING DRAINAGE STRUCTURE.
- 5 INSTALL UNDERDRAIN TRENCH CLEANOUT. SEE DETAIL 6 ON SHEET DD1.
- 6 INSTALL RECTANGULAR SLIP RESISTANT SOLID METAL COVER PER MDSOT DETAIL P-3040-02.
- 7 CONCRETE HEADWALL. SEE DETAIL 1 ON SHEET DD1.
- 8 BASIC DEPRESSION. SEE SHEET CS3.
- 9 CURB AND RAIL OUTFALL PROTECTION. SEE DETAIL 3 ON SHEET DD1.
- 10 ADJUST CATCH BASIN.

LEGEND:



CITY OF SAMMAMISH APPROVAL

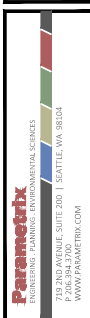
City Engineer	Date
Community Development	Date

60% REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

SHEET NO.
51 OF 135

PLAN AND PROFILE

PROJECT NAME
EAST LAKE SAMMAMISH MASTER PLAN TRAIL SOUTH SAMMAMISH SEGMENT B
SAMMAMISH, WA



ONE INCH AT FULL SCALE, IF NOT SCALE ACCORDINGLY
 DRAWN BY: PURGAMAN
 CHECKED BY: P. JOHANNESSEN
 SCALE: 1"=20'
 DATE: 09/28/2016

REVISIONS	DATE	BY

APP'D BY: P. JOHANNESSEN

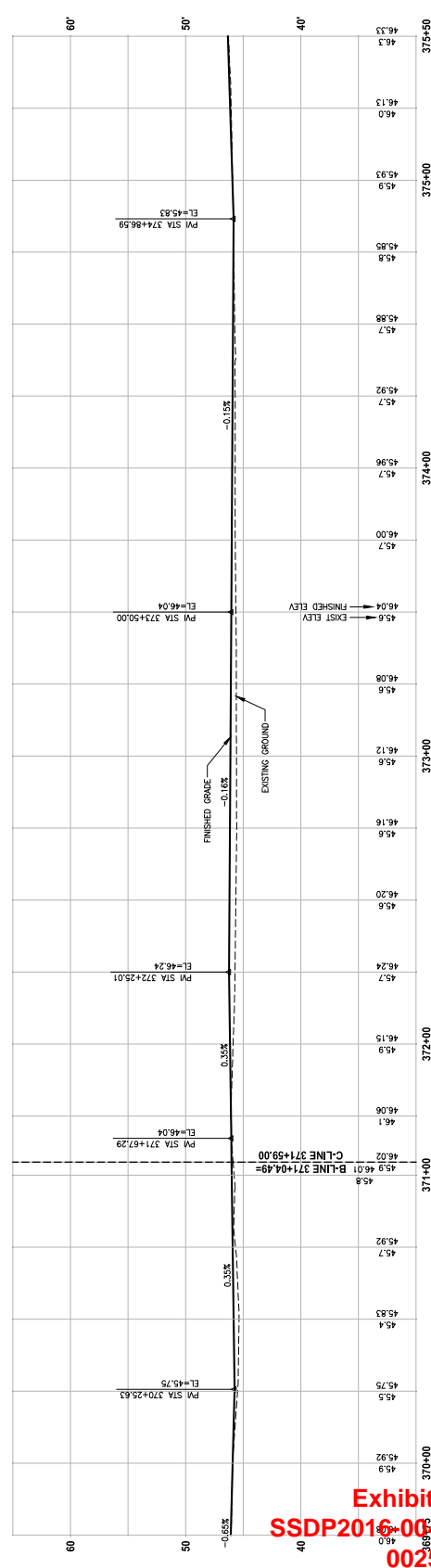
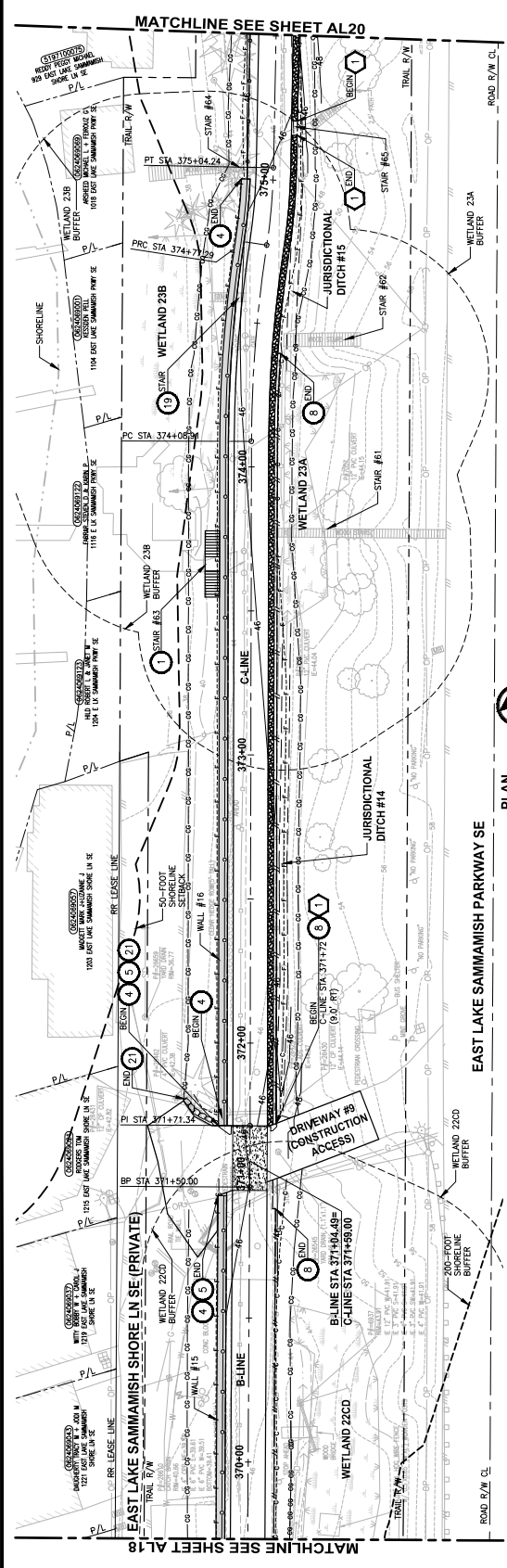
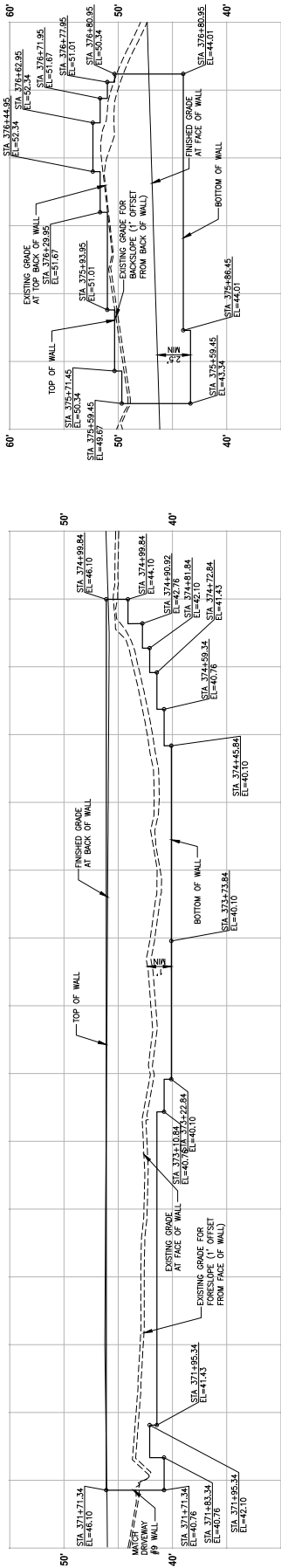
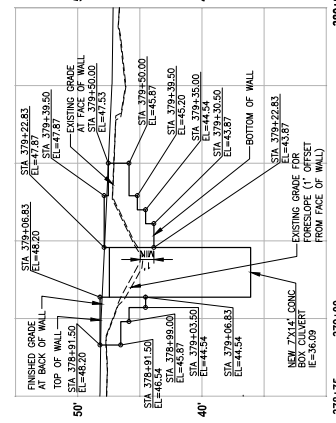


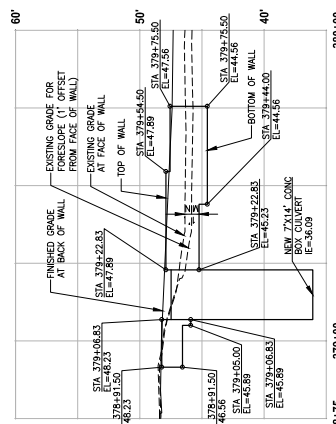
Exhibit 29
SSDP2016-00415
002516



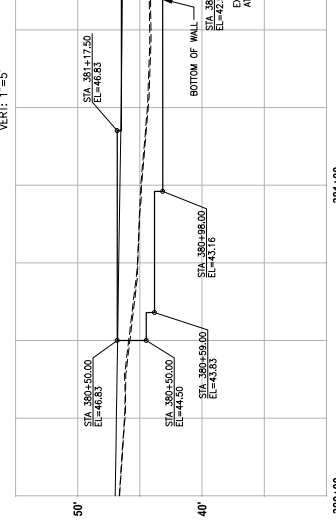
371+50 372+00 373+00 374+00 374+00 375+00 375+25 375+50 376+00
LEFT STRUCTURAL EARTH WALL WALL #16 (FILL) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'



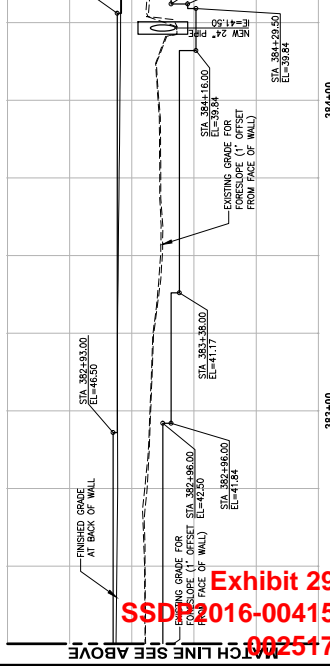
378+75 379+00 380+00
LEFT STRUCTURAL EARTH WALL WALL #18 (FILL) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'



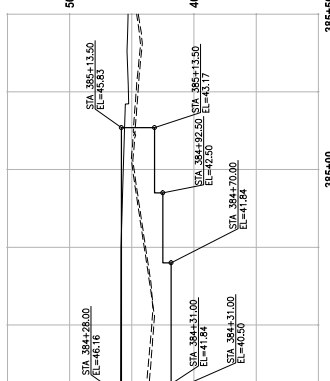
378+75 379+00 380+00
RIGHT STRUCTURAL EARTH WALL WALL #19 (FILL) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'



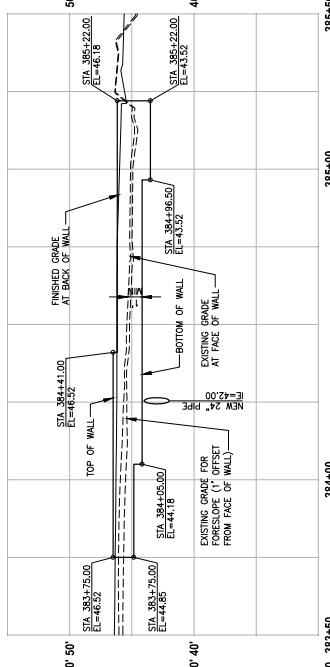
381+00 382+00
LEFT STRUCTURAL EARTH WALL WALL #20 (FILL) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'



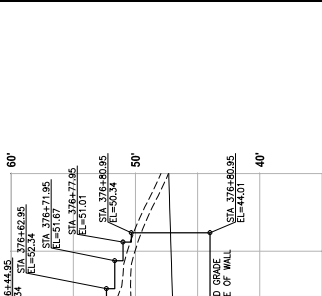
383+00 384+00
LEFT STRUCTURAL EARTH WALL WALL #20 (FILL) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'



384+00 385+00
RIGHT STRUCTURAL EARTH WALL WALL #21 (FILL) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'



385+00 386+00
RIGHT STRUCTURAL EARTH WALL WALL #22 (FILL) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'



376+00 377+00
RIGHT GRAVITY BLOCK WALL WALL #17 (CUT) C-LINE
 HORIZ: 1"=20' VERT: 1"=5'

MATCH LINE SEE BELOW

NOTES:

- UTILITIES SHOWN ARE APPROXIMATE. CONTRACTOR TO VERIFY BY EXCAVATIONS AND DEPTH, AS AUTHORIZED BY THE ENGINEER.
- PVC SEPIPE 4 IN. DIA. 10 LF CENTERED AT WALL. SEE SPECIAL PROVISIONS.
- BUILDING PERMIT IS REQUIRED FOR WALLS OVER 4 FEET HIGH (FROM TOP TO BOTTOM) AND WALLS SUPPORTING A SURCHARGE.
- ELEVATIONS AT STARS ALONG FACE OF TRAIL WALL SHALL BE GREATER THAN TOP OF THE TRAIL WALL ELEVATION.
- DETAILED LAYOUT FOR WALL STIFFERS AND WALLS FOR STIFFERS ARE NOT COMPLETE. YET ELEVATIONS SHOWN ARE PRELIMINARY AND ARE SUBJECT TO CHANGE.

CITY OF SAWMAMISH APPROVAL

City Engineer	Date
Community Development	Date

60% REVIEW SUBMITTAL
 NOT FOR CONSTRUCTION

SHEET NO. 102 OF 135
WP6

WALL PROFILES

PROJECT NAME
EAST LAKE SAWMAMISH MASTER PLAN TRAILS SOUTH SAWMAMISH SEGMENT B
 SAWMAMISH, WA



DATE: _____ BY: _____
 CHECKED: _____
 APPROVED: _____
 DATE: _____

DATE: _____ BY: _____
 CHECKED: _____
 APPROVED: _____
 DATE: _____

DATE: _____ BY: _____
 CHECKED: _____
 APPROVED: _____
 DATE: _____

Exhibit 29
 SSD# 2016-00415
 0025157

CONSTRUCTION NOTES:

- 1 REMOVE LAWN IN THIS AREA.
- 2 REMOVE CONCRETE PAD.
- 3 REMOVE GRAVEL PAVING.
- 4 REMOVE STRUCTURE IN THIS AREA.
- 5 PROTECT EXISTING NATIVE TREES AND SHRUBS; REMOVE BLAGRASS AND OTHER UNWANTED PLANT WITH NATIVE WETLAND PLANTS AND PLACE WOOD CHIP MULCH OVER ENTIRE AREA.
- 6 REMOVE BLAGRASS AND OTHER UNWANTED; INVASIVE PLANTS; AMEND SOIL WITH COMPOST; WOOD CHIP MULCH OVER ENTIRE AREA AND PLACE WETLAND PLANTS.
- 7 GRADE AREA TO CREATE WETLAND CONDITIONS AND AMEND SOIL WITH COMPOST. PLANT WITH NATIVE WETLAND PLANTS.

GENERAL NOTE:

1. SEE SHEET LA03 FOR DETAILS AND PLANT LISTS.
2. SEEINGS FOR REMOVED DRIVEWAYS IS NOT PART OF THE MITIGATION PLAN.

LEGEND:

- WBE WETLAND BUFFER ENHANCEMENT
- WBA WETLAND BUFFER ADDITION AREA
- SSE SHORELINE SETBACK ENHANCEMENT AREA
- WCW/R WETLAND CREATION OR RESTORATION AREA
- WE WETLAND ENHANCEMENT AREA
- SBE STREAM BUFFER ENHANCEMENT AREA
- SEEINGS FOR REMOVED DRIVEWAYS.
- NOTE 2.

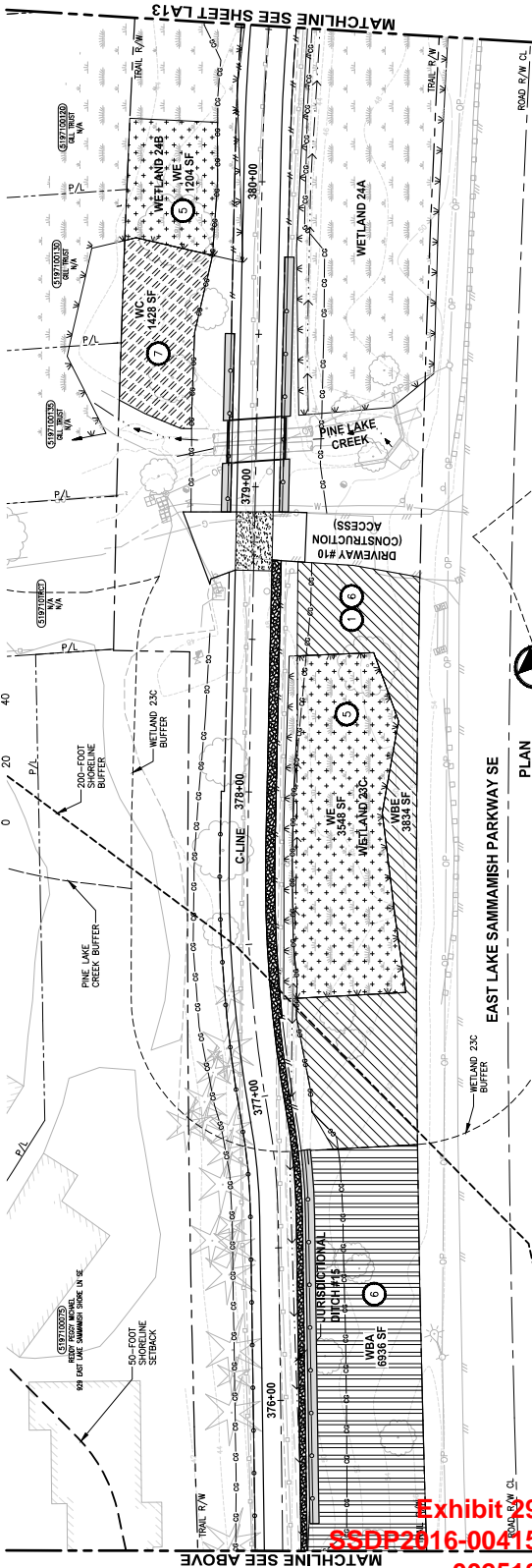
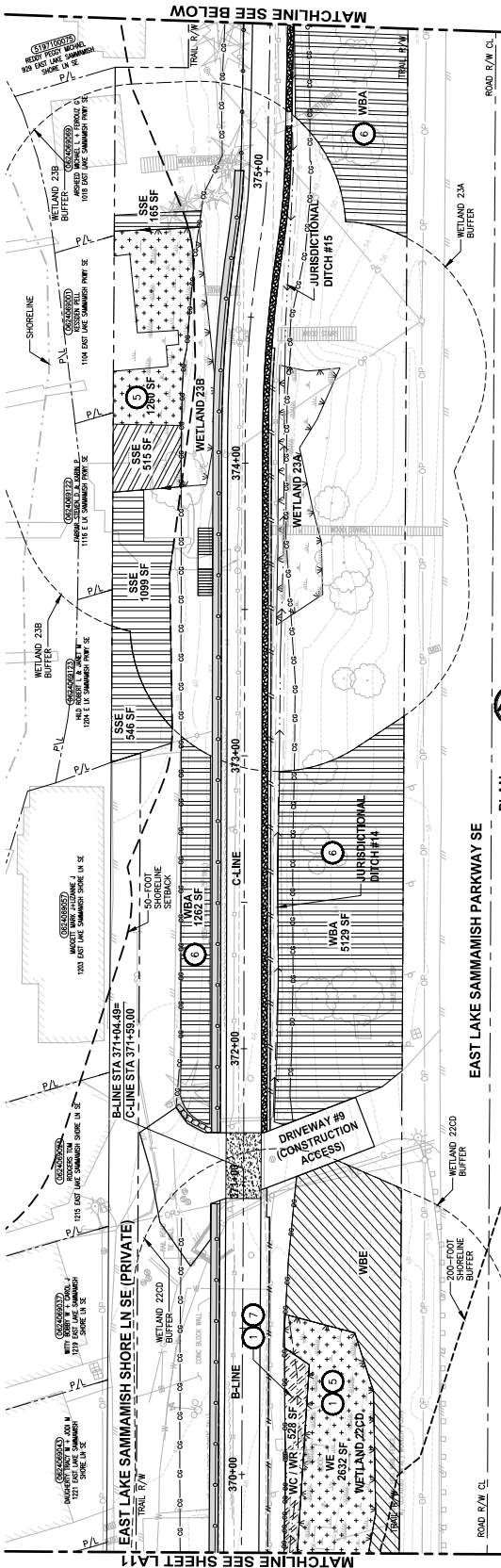


Exhibit 09
SDP2016-00415
002518

CITY OF SAMMAMISH APPROVAL	
City Engineer	Date
Community Development	Date

60% REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

SHEET NO.	124 OF 135
PROJECT NAME	EAST LAKE SAMMAMISH MASTER PLAN TRAILS SOUTH SAMMAMISH SEGMENT B
DATE	SEPTEMBER 2016

LANDSCAPE PLAN

PROJECT NAME
EAST LAKE SAMMAMISH MASTER PLAN TRAILS SOUTH SAMMAMISH SEGMENT B
SAMMAMISH, WA

Parametrix
Distribution Planning Environmental Sciences
7525 2ND AVENUE, SUITE 200 | EASTLYN, WA 98024
P: 206.394.4700
WWW.PARAMETRIX.COM

WETLAND ARCHITECT
J. SWENSON
JENS SWENSON
LICENSE NO. 646
EXPIRES ON _____

ONE INCH AT FULL SCALE
IF NOT SCALE ACCORDINGLY
E:\121075\2016\03\LA-03
384_1521-075_P19_T03
APP'D: J.D.C.
SEPTEMBER 2016

REVISIONS	DATE	BY

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- [Property Tax Bill](#)
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- [Glossary of Terms](#)
- [Area Report](#)
- [Property Detail](#)

PARCEL

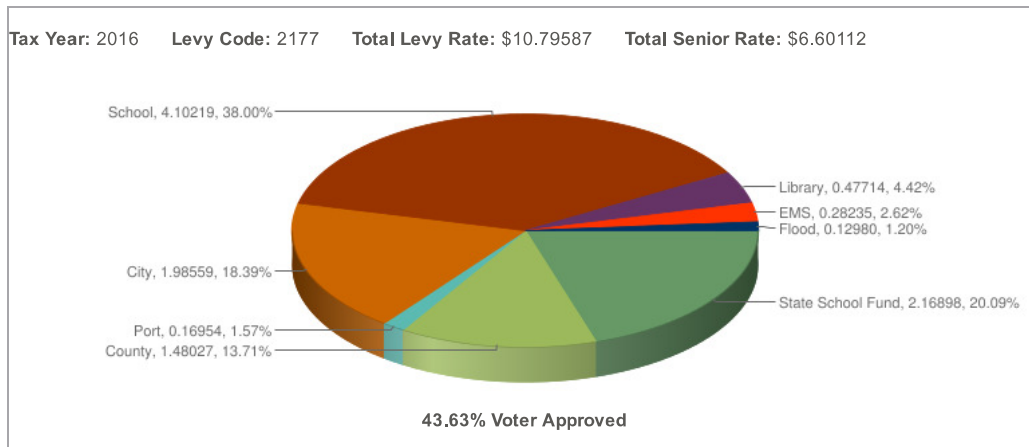
Parcel Number	062406-9123
Name	HILD ROBERT L & JANET M
Site Address	
Legal	S 58.70 FT OF POR OF GL 1 LY WLY OF NP R/W & SH LDS ADJ

BUILDING 1

Year Built	
Total Square Footage	
Number Of Bedrooms	
Number Of Baths	
Grade	
Condition	
Lot Size	804
Views	Yes
Waterfront	LAKE SAMM



TOTAL LEVY RATE DISTRIBUTION



[Click here to see levy distribution comparison by year.](#)

TAX ROLL HISTORY

Valued Year	Tax Year	Appraised Land Value (\$)	Appraised Imps Value (\$)	Appraised Total (\$)	Taxable Land Value (\$)	Taxable Imps Value (\$)	Taxable Total (\$)
2016	2017	100,000	48,000	148,000	100,000	48,000	148,000
2015	2016	100,000	45,000	145,000	100,000	45,000	145,000
2014	2015	100,000	39,000	139,000	100,000	39,000	139,000
2013	2014	100,000	18,000	118,000	100,000	18,000	118,000
2012	2013	100,000	10,000	110,000	100,000	10,000	110,000
2011	2012	100,000	10,000	110,000	100,000	10,000	110,000
2010	2011	156,000	0	156,000	156,000	0	156,000
2009	2010	156,000	0	156,000	156,000	0	156,000

ADVERTISEMENT

ADVERTISEMENT

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Monday, February 6, 2017 10:12 AM
To: Nick Tsilas
Subject: Re: Mint Grove Residents Joint Comments on Section 2B East Lake Sammamish Trail Segment

Dear Nick,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: Nick Tsilas <ntsilas@microsoft.com>
Sent: Friday, January 27, 2017 4:07 PM
To: Lindsey Ozbolt
Cc: cjwitty@live.com; Kristin Landry; Tom Rodgers; wsualum@comcast.net; mintgrove@comcast.net; dgb18@comcast.net; saramathy@yahoo.com; gbreuel@msn.com; thornish67@gmail.com; carolinekaufman@yahoo.com; vernlindquist@msn.com; darrenpritt@hotmail.com; Charliwright3@gmail.com; hettich7@comcast.net; johnlandry@southernwine.com; Jane Tsilas; christensenba@hotmail.com; Tracy Daugherty; lizlablvr@aol.com
Subject: Mint Grove Residents Joint Comments on Section 2B East Lake Sammamish Trail Segment

Dear Ms. Lindsey Ozbolt – on behalf of the residents of Mint Grove, please find our joint comments. First attachment is a signed, scanned copy. Second is the digital. Thanks,

Re: Mint Grove Residents – Joint Comments and Questions on Section 2B East Lake Sammamish Trail Segment

January 27, 2017

Sent Via E-mail

To: Lindsey Ozbolt - lozbolt@sammamish.us

Dear Ms Ozbolt – We the residents of Mint Grove, identified in the [South Sammamish Plan B 60% Design Plan](#) as 361+00, 362+00, 363+00, 364+00, 365+00, 366+00, 367+00, 368+00, 369+00, 370+00, 371+00, 372+00, 373+00 on sheets 49-51 submit the following comments.

- **Summary:** Safety and access to our homes is our number one concern. We support the responsible and thoughtful paving of the trail so that it does not make a narrow and hazardous lane (East Lake Sammamish Shore Lane, SE) even more narrow and hazardous. **To that end we request the center line of the trail be moved to the east in the areas adjacent to Mint Grove so that the trail does not narrow the lane in any way. We ask that the SSDP approval be put on hold until the 90% plans are released and our concerns have been fully addressed and incorporated into the 90% design review.**
- Mint Grove is a small, dead-end residential neighborhood with a lane that does not support simultaneous bi-directional traffic. The lane is so narrow that larger vehicles such as moving trucks and recycling trucks cannot navigate the lane. Because of this, certain curbside services such as recycling and yard waste are not provided to Mint Grove residents, and access by other services such as delivery, construction, and emergency vehicles is extremely challenging. Access to and from our homes is also especially challenging because turns and parking are very tight, and angles are sharp.



Pictures at various sections of East Lake Sammamish Shore Lane, SE showing narrow lane (looking south).

- The proposed 60% plan creates a more dangerous living environment by considerably narrowing the lane and potentially significantly slowing or completely blocking emergency vehicle and services vehicle access to our homes. **In addition to moving the center line east so that the lane is not narrowed, we specifically request Eastside Fire & Rescue to review our comments and make an on-site assessment of the proposed C&G fencing location during construction and post construction results.**

- The 60% draft plan unnecessarily moves the center line west at Mint Grove (see pics below) considerably narrowing the lane and creating a dangerous and untenable situation for Mint Grove residents. Almost all our safety and access concerns are addressed if the plan is revised so that the center line of the trail is moved east so that East Lake Sammamish Shore Lane, SE is not narrowed in any way. Doing this may be the difference between life and death in an emergency and will allow residents to continue to receive the services they currently receive. Doing this will also save numerous trees.



Pictures of trail and center line looking south showing center line being moved to west, and showing ditch and shrubbery to east where center line can easily be moved to with.

- There is plenty of land in the easement and there will be no adverse impact in moving the center line of the trail to the east (currently shrubbery and ditch). In fact, just immediately to the north of Mint Grove – the 60% plan provides for the center line being moved substantially to the east (approx. 6 ft). See 375+00 through 376+04. Note that ditch (alleged “wetland”) in this area is the same as the ditch adjacent to the trail by Mint Grove. See pics below.

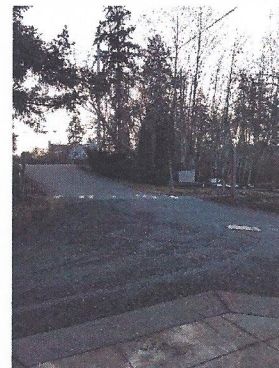
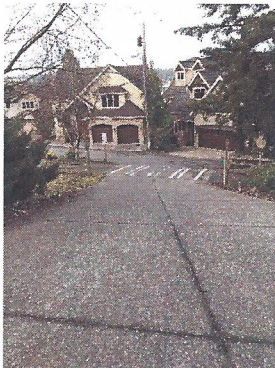


Pictures of sections 375+00 through 376+04 just north of Mint Grove looking south showing that center line of trail is moved east by approx. 6 ft into the area of the ditch and shrubbery.

- During our meetings with Kelley Donahue of the King County Department of Natural Resources, we were told that the current schedule for the construction of South Segment B is for 2 years. This will result in C&G fencing being in place and disrupting access to residents and placing increase risk to residents in an emergency. If the center line is moved to the west as indicated in the 60% draft plan access to our homes will be severely impacted and in some cases residents may not have access at

all. This is one more reason to address our concerns and move the center line east so that the road is not impacted in any way. Assuming our concerns regarding the center line are addressed, we also want to confirm that access to our homes will not be unduly impacted. We request South Segment B be broken into two phases which will significantly reduce the time frame residents are impacted by the construction, and that in any case that the impact to Mint Grove residents is reduced to an industry acceptable time frame of a few months.

- The current plans show a design which modifies our neighborhood entrance which changes the grade/slope of the entrance both prior to and after meeting the trail surface. It appears from the plans that the entrance surface to the east of the trail will be re-graded and re-surfaced. At much expense to the residences of Mint Grove this surface area was updated in 2002 with very thick concrete including rebar to support various vehicle types, including delivery, garbage, and construction trucks, and the concrete surface has a heavy brushed surface to improve traction. The current ELST plans do not show the re-grading of area being re-surfaced with same level of materials as will be disturbed by King County. At one of the meetings with the King County representative for a half hour informational review we were advised the replacement materials will be concrete on the trail surface, but asphalt in all other areas. The use of asphalt on these inclines presents a dangerous situation. The existing slope of the entrance to Mint Grove is at 22.8 degrees and will be increased to 26.18 degrees. The residents of Mint Grove currently pull their 96-gallon recycling bins and 96-gallon yard waste bins up to the Lake Sammamish Parkway weekly for these bins to be emptied by Republic Services, and then later bring them back down this steep incline. By increasing the slope and laying asphalt this will cause the slope to be slippery and could result in injuries to residents. For safety reasons, we request that the slope is not increased and that the same level of materials currently in place by used by King County.



Pictures of driveway and entrance into Mint Grove and East Lake Sammamish Shore Lan, SE showing grade and materials.

Bobby W. Wittly
Carol Wittly

1-27-17 at 1219 E. Lk. Samm Sar Ln SE

1-27-17 at 1219 E. Lk. Samm. Str. Ln. SE.

- In addition to the safety issue noted above, we feel it is the county's responsibility to repair any damage caused by the trail construction and restore the entrance to its original condition, including materials and workmanship. The entrance to Mint Grove is a private driveway owned by the Mint Grove residents and it is currently labeled on the 60% plans as a construction access. King County has not requested approval from the residents of Mint Grove to use this private lane. The plans should be revised to reflect the entrance to Mint Grove as a private driveway, and the private driveway entrance into the lane should be restored to its original condition.

Sincerely, Mint Grove Residents

JAY & CAROLINE KAUFMAN - 1409

Jane Tsilas
JANE + NICK TSILAS 1429

01/27/17

Christi
Mike & Christi Mettich - 1419 01/27/17

GEORGE BREUEL George Breuel 1415 1-27-17

Angela Burrell Lori Burrell 1317

Robert Christensen Ann Christensen 1309

Grace Easten 1327

Vern Lindquist 1246

Jeanne Lindquist 1241

Tyler 1220

Darren Pruitt - 1435
Nick Tsilas

1225
Tom and Suzanne Hornish / Nick Tsilas 1423

SB-50 Charlie Wright / Nick Tsilas - 1411

Exhibit 29
SSDP2016-00415
002524

Re: Mint Grove Residents – Joint Comments and Questions on Section 2B East Lake Sammamish Trail Segment

January 27, 2017

Sent Via E-mail

To: Lindsey Ozbolt - lozbolt@sammamish.us

Dear Ms Ozbolt – We the residents of Mint Grove, identified in the [South Sammamish Plan B 60% Design Plan](#) as 361+00, 362+00, 363+00, 364+00, 365+00, 366+00, 367+00, 368+00, 369+00, 370+00, 371+00, 372+00, 373+00 on sheets 49-51 submit the following comments.

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Exhibit 29
SSDP2016-00415
002525

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Sincerely, Mint Grove Residents

Lindsey Ozbolt

From: Lindsey Ozbolt
Sent: Monday, February 6, 2017 10:11 AM
To: Mark Cross
Subject: Re: Comments by Mark Cross regarding Shoreline Permit for Trail Segment 2B

Dear Mark,

Thank you for contacting the City of Sammamish regarding the current Shoreline Substantial Development Permit Application for East Lake Sammamish Trail Segment 2B (SSDP2016-00415).

Your comments have been received and will be included in the project record. At the close of the comment period, all comments will be compiled and provided to King County for review and response. You will be included in future notices the City issues for this proposal.

Regards,

Lindsey Ozbolt
Associate Planner | City of Sammamish | Department of Community Development
425.295.0527

From: Mark Cross <markcross6616@gmail.com>
Sent: Friday, January 27, 2017 4:07 PM
To: Lindsey Ozbolt
Subject: Comments by Mark Cross regarding Shoreline Permit for Trail Segment 2B

Lindsey,
please see attached comments. I am submitting comments on the Shoreline Permit for the King County Parks Trail segment 2B.

Thank you for your work on this important project.

Mark Cross

January 25, 2017

MS. Lindsey Ozbolt
Associate Planner
City of Sammamish
98075

SUBJECT: Comments Regarding East Lake Sammamish Trail Segment 2B – SSDP2016-00415

MS. Ozbolt,

Thank you for the opportunity to comment on this critical trail segment in Sammamish. I have visited the trail segment on many occasions and have looked at the plan set at City Hall. I support approval of the Shoreline Permit for the construction of trail segment 2B for the very reasons I supported the opening of the trail to public use 16 years ago.

1. CONCSISTNECY WITH SHORELINE MANAGEMENT ACT AND LOCAL SMP

RCW90.58.020 “The legislature declares that the interest of all of the people shall be paramount in the management of shorelines of statewide significance. The department, in adopting guidelines for shorelines of statewide significance, and local government, in developing master programs for shorelines of statewide significance, shall give preference to uses in the following order of preference which:

- (1) Recognize and protect the statewide interest over local interest;
- (2) Preserve the natural character of the shoreline;
- (3) Result in long term over short term benefit;
- (4) Protect the resources and ecology of the shoreline;
- (5) Increase public access to publicly owned areas of the shorelines;
- (6) Increase recreational opportunities for the public in the shoreline;
- (7) Provide for any other element as defined in RCW 90.58.100 deemed appropriate or necessary.”

The trail represents public use of public property to provide recreational opportunities, non-motorized transportation connectivity and public access to the Shoreline and visual access to waters of the State of Washington consistent with State law. I think that developing the historic rail transportation corridor as a public recreational walking and biking facility, consistent with modern design standards, carries out the State Shoreline guidance provided by the State Legislature that “The legislature declares that the interest of all of the people shall be paramount in the management of shorelines of statewide significance.” **RCW 90.58.020**

Particularly as you go down this list of guidelines, increasing public access to publicly owned areas of the shorelines and increasing recreational opportunities for the public in the areas of the shoreline, support trails in general and the completion of segment 2B of the East Lake Sammamish trail, as designed and including required mitigation is consistent with the Legislative guidance.

Exhibit 29
SSDP2016-00415
002530

2. SEGMENT 2B DESIGN NEEDS TO PROVIDE ACCESS FOR ALL PARTIES WITH EASEMENTS TO WATERFRONT

I request that both the City and County acknowledge that not only lakeside residents have existing easements and property rights along Lake Sammamish. Sammamish resident Mary Wictor is providing the legal documents that confirm that platting of many of the hillside subdivisions above the lake were provided with easements to access lakeside properties. I request that the design of Segment 2B include gates/ access so that existing access easements, granted to upland property owners, can continue to be used.

3. TRAIL PLUS TWO FULL SERVICE TRAIL HEAD/PARKING LOTS IS HUGE PUBLIC ASSET

The King County Parks design for segment 2B includes a trail head with parking for 28 cars at the bottom of Inglewood Hill Road. This trail head will include access for all trail users including handicapped residents and visitors as well as the many families here in Sammamish. Sammamish residents will also have access to a future parking lot and handicap access ramp to the East Lake Sammamish Trail across from the 7-11. This parking lot along with the detailed plans we have for the Inglewood Hill Trail head and the current City of Sammamish Park at Sammamish Landing will provide close to 100 parking spaces for residents to use for access to the regional trail. The two proposed King County Parks trail heads with parking and bathrooms along with the City of Sammamish Parks Sammamish Landing facility will give residents of the City and the Region with a tremendous passive and active park system and link to the region.

4. SEGMENT 2B DESIGN INCLUDES KOKANEE CULVERTS – NEEDED BY FALL 2018

I request that the City of Sammamish move to approve the Shoreline Permit for segment 2B so that the project can be constructed, including the fish friendly culverts for Zaccouse and Ebright Creeks during the summer of 2018 in time for the fall return of the Kokanee. The City Council has directed over a million dollars to improving fish access under the East Lake Sammamish Parkway. It would be a shame if the County cannot construct and have in place the two fish passable culverts in place for the fall 2018 Kokanee spawning season.

5. CITY CAN SUPPORT PLANNED TRAIL ACCESS BY ADDING SIDEWALK ON THOMPSON ROAD

The design for segment 3B includes a pedestrian ramp just to the south of Thompson Hill Road. This will be a neighborhood and resident amenity that will draw pedestrians and bicyclists to the base of Thompson Hill road. I request that the City include a sidewalk up the very first segment of Thompson Hill Road in the CIP for construction in the future. As Sammamish has developed over the 30 years that I have lived here (May of 1987), vehicular, pedestrian, and bicycle traffic have all increased on Thompson Hill Road. One of my neighbors had a bike accident at the bottom of Thompson Hill road. The time is approaching where all modes of transportation and recreational walkers need a street design to keep everybody safe.

6. BILL WAY IDENTIFIED ADDITIONAL STREAM FOR FUTURE KOKANEE PROJECT

Sammamish resident Bill Way first pointed out to me a year round, unnamed stream that runs along a portion of trail segment 2B just south of Inglewood Hill Road and then under the 2B segment trail alignment to Lake Sammamish through a community beach property. I think that Bill has the long professional experience with stream habitat projects such that this stream be identified for future salmon recovery efforts. I request that the design of segment 2B take into consideration the future rehabilitation of this stream segment as a part of King County and the City of Sammamish's commitment to Kokanee recovery. Bill Way has submitted detailed locational information with his comments.

Thank you for the opportunity to comment on the Shoreline Permit for Segment 2B of the East Lake Sammamish Trail. This project, will complete and connect a regional trail that will have a large long term benefit to Sammamish residents and residents of the region.

Mark Cross,



247-208th Ave NE,
Sammamish WA. 98074

railway was used for Timber, there were owners or companies that likely needed the WATER to do their business. So where Sammamish juts out the most to the West, the ROW easements/land are 100' on EACH SIDE of the railway/trail center line... for a total whopping 200 feet in ROW width!

I have also learned that the level of Lake Sammamish has changed over years, not only naturally, but also via manmade efforts. I believe in 1962 the Army Corps of Engineers worked to lower the Lake level, by draining it (north end near Redmond). This impacted the shoreline location and low, mid, high water marks (at least in practice/reality if not also legally). In this way, property owners on the Lake have been "growing" their lot(s) over time. Regardless, I understand that Lake Sammamish is waters are State waters and public has access. I also know that the Trail is voter approved and is using public funds to do the project for this Public Amenity.

Thus, as the K.C. ELST 60% design undergoes review, it is really critically important to look at this situation... and provide individual or groups residents/owners with easements any accesses they are due. {It is not fair to just deal with lake-side homes/houses.} Also, on a much wider scale is the concept of Public Access to/for Public Lands. If the old Railroad ROW now owned by King County is PUBLIC... all the way to the edge of the ROW... then the Public (including City of Sammamish plus those using this section for the regional trail system) should have rights to go to the lake, view it, put a toe in the water, enjoy lunch, etc... if not also Beach & Swimming.

Furthermore, the View Point Park location is a very nice little beach area. It used to be open--or at least not fenced off. But I believe was fenced (saying "Members Only") probably around the time the gravel soft-path K.C. ELST Trail was to be installed/completed. It does appear from looking at maps and history, that these private residents and homes are enjoying use of public areas and lands, encumbered either by the King County easement/RailRoad or actual ownership of the land in the full ROW by King County through the purchase of the RR and ROW since 1998, I believe.

So there is the situation of encumbered ROW areas and easements, and potential or actual encroachments that existing within the ROW. If a solution/resolution is to allow encroachments to continue to exist (not make people move their houses which were permitted under King County... or the City of Sammamish), then they must not get any bigger nor more obtrusive into the ROW. Things that can be removed also show be move if they encroach. Also, Public Use of the Public Lands, including the full ROW and access(es) to Lake Sammamish should looked at and decided, hopefully for wider use to those in the area, City, or even the whole County or Region itself, and visitors too.

I know this issue is a big one. I hope that by providing some of these old, historical documents via available online from King County, this may help resolve the issues to the benefit of everyone. Protecting private land owners, allowing easements to have the access they should, and including Public Access of trail, beach, and lake wherever and whenever it could be made must be discussed, settled, and implemented sooner than later.

Respectfully,
Mary Victor, Sammamish resident since 6/2000.

VIEW POINT PARK

Section 6, Twp. 24 N. R 6 E. W.M.

May 1947

SHEET 2 OF 2 SHEETS

Scale: 1 in. = 100 feet.

General Engineering Co. Inc.
Engineers.

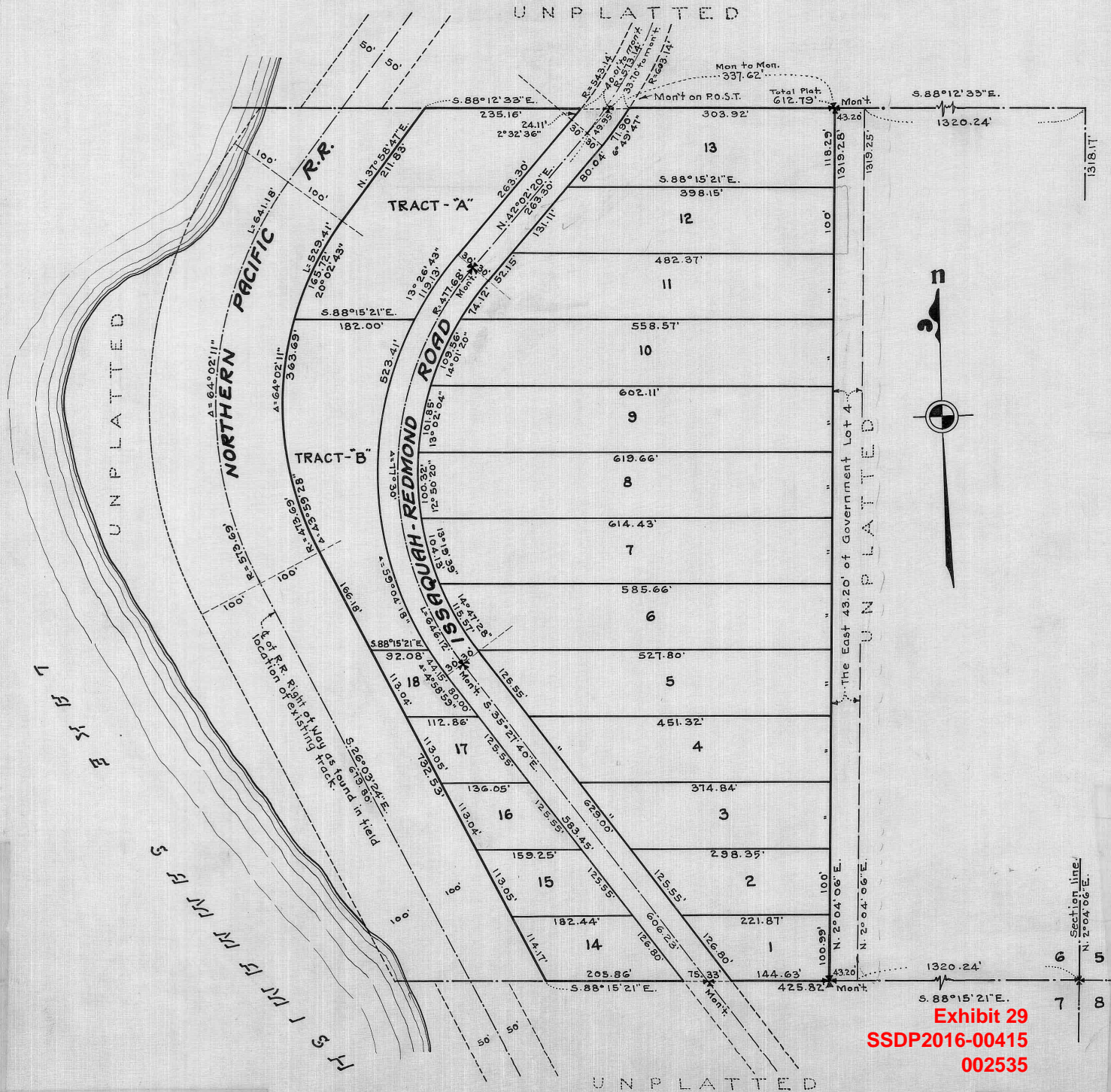
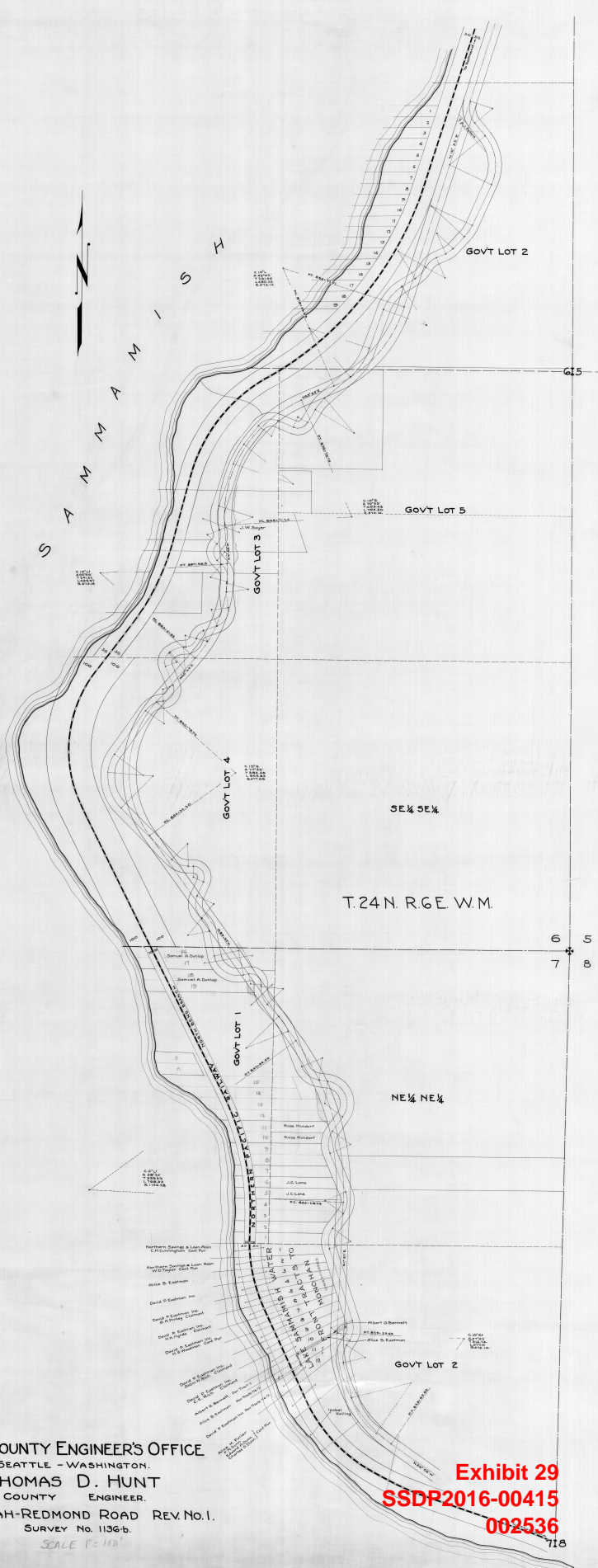


Exhibit 29
SSDP2016-00415
002535

(8661) 46-29C

L A K E

S A M M I S H



KING COUNTY ENGINEER'S OFFICE
 SEATTLE - WASHINGTON.
THOMAS D. HUNT
 COUNTY ENGINEER.
 ISSAQUAH-REDMOND ROAD REV. No. 1.
 SURVEY No. 1136b.
 SCALE 1" = 150'

Exhibit 29
SSDP2016-00415
002536



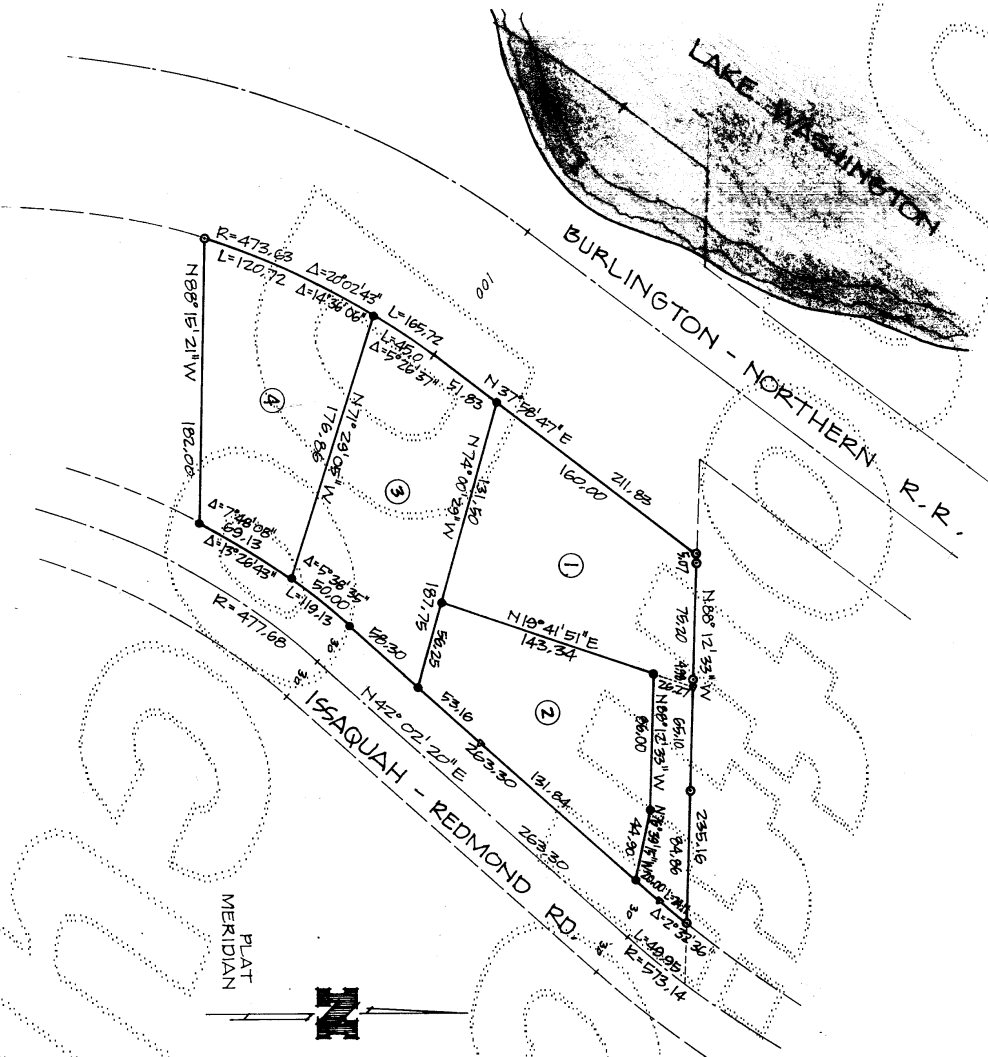
HART, WILLIAMS & ROTH, INC.
SURVEYORS · ENGINEERS
KIRKLAND, WASHINGTON 208.822.4171

PROPERTY SURVEY
TRACT 'A', VIEW POINT PARK, 1 SEC. 6, T12W, 24N R. 6E - KING COUNTY, WASHINGTON
BUTLER & WALLS 531 LYON BLVD, SEATTLE

Recorder's Certificate No. 7311290306
Filed for record this 28 day of NOV 1973
01806544. In Book 1 of SUBJECTS of 1971-74
of the county of WILKINSON, WASHINGTON
BY *James A. Butler*
JAMES A. BUTLER
SURVEYOR

APPROVED	SCALE
DATE	11" = 50'
NOV 15 1973	FIELD BK.
	80:85
	7281

SURVEYOR'S CERTIFICATE
This map correctly represents a survey conducted by Hart, Williams & Roth, Inc. under my supervision, in conformance with the requirements of the Survey Recording Act of the request of BUTLER & WALLS in ORDER 1973
James A. Butler
JAMES A. BUTLER
SURVEYOR



LEGEND
 ○ EXISTING IRON PIPE (SET BY KING CO.)
 ▲ CAPPED RE-BAR
 ● HUB

LEGAL DESCRIPTION
TRACT 'A' OF VIEW POINT PARK, ACCORDING TO PLAT THEREOF RECORDED IN VOLUME 44 OF PLATS, PAGE 35 AND 36, RECORDS OF KING COUNTY, WASHINGTON.

AI MARTIN ASSESSOR'S

SHORT SUBDIVISION APPLICATION

KING COUNTY, WASHINGTON



LAND USE MANAGEMENT DIVISION
Department of Community Development
W. 217 King County Court House
Seattle, Washington 98101

Telephone 344-4292

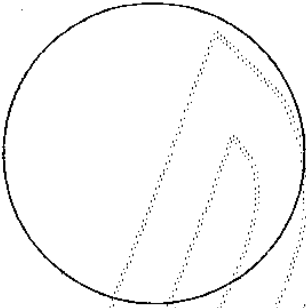
940510-1810 DE-36-08 PM KING COUNTY RECORDS 002 19

OWNER'S NAME <i>Dora LaPlant</i>		STREET		CITY		ZIP	PHONE	
BUTLERS & WALLS		551 Lyon Building		Seattle		98104	685 9463 855 3843 MA 2 008	
AGENT'S NAME		STREET		CITY		ZIP	PHONE	
HART, WILLIAMS & ROTH		909 Kirkland Avenue		Kirkland		98033	VA 2 417	
1/4	1/4	SEC	TWP	RGE	TAX LOT	DATE OF SEGREGATION	ACREAGE	NO. OF LOTS
SW	SE	6	24	6		<i>old plat</i>	1.56 Ac.	4
WATER SOURCE			SEWAGE DISPOSAL		SIGNATURE (I CERTIFY THAT THE INFORMATION FURNISHED BY ME IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE.)			35.00
W.D. #82			Septic Tank		<i>J. Birch</i>			

A 03158

DO NOT WRITE BELOW THIS LINE

9405101810



KROLL PAGE <i>547 E</i>	RECEIVED DATE RECD <i>4/6/79</i>	773058
BOOK <i>14</i>		
ZONING <i>R.S. 91000 Pot Rm1800</i>		
RELATED FILE		

	ACTION	INITIALS	DATE
PRELIMINARY			
FINAL	<i>Approved</i>	<i>AL</i>	<i>8-28-73</i>

APPROVED
[Signature]
LAND USE MANAGEMENT

CONCURRENCE	DATE
Public Works	
Health Department	

CONDITIONS OF FINAL APPROVAL / REASONS FOR DISAPPROVAL

1. STANDARD CONDITIONS

FILED BY CHICAGO TITLE INSURANCE CO.
REF. # *43365-7*

Exhibit 29
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002538

02557

MAP OF 18100

TRACT A OF VIEW POINT PARK
ACCORDING TO PLAT THEREOF
RECORDED IN VOLUME 44 OF
PLATS, PAGE 35 AND 36, RECORDS
OF KING COUNTY, WASHINGTON.

LEGEND
● EXISTING IRON PIPE (SET BY KING CO.)
● CAPPED RE-BAR
● HUB

Peter Adland

SURVEYOR'S CERTIFICATE
This was correctly replatted & later corrected
by Hart, Williams & Roth, Inc. 4/15/79
in accordance with the
instructions of the Survey Record of 4/1
of the record of FALLS VALLEY
1972



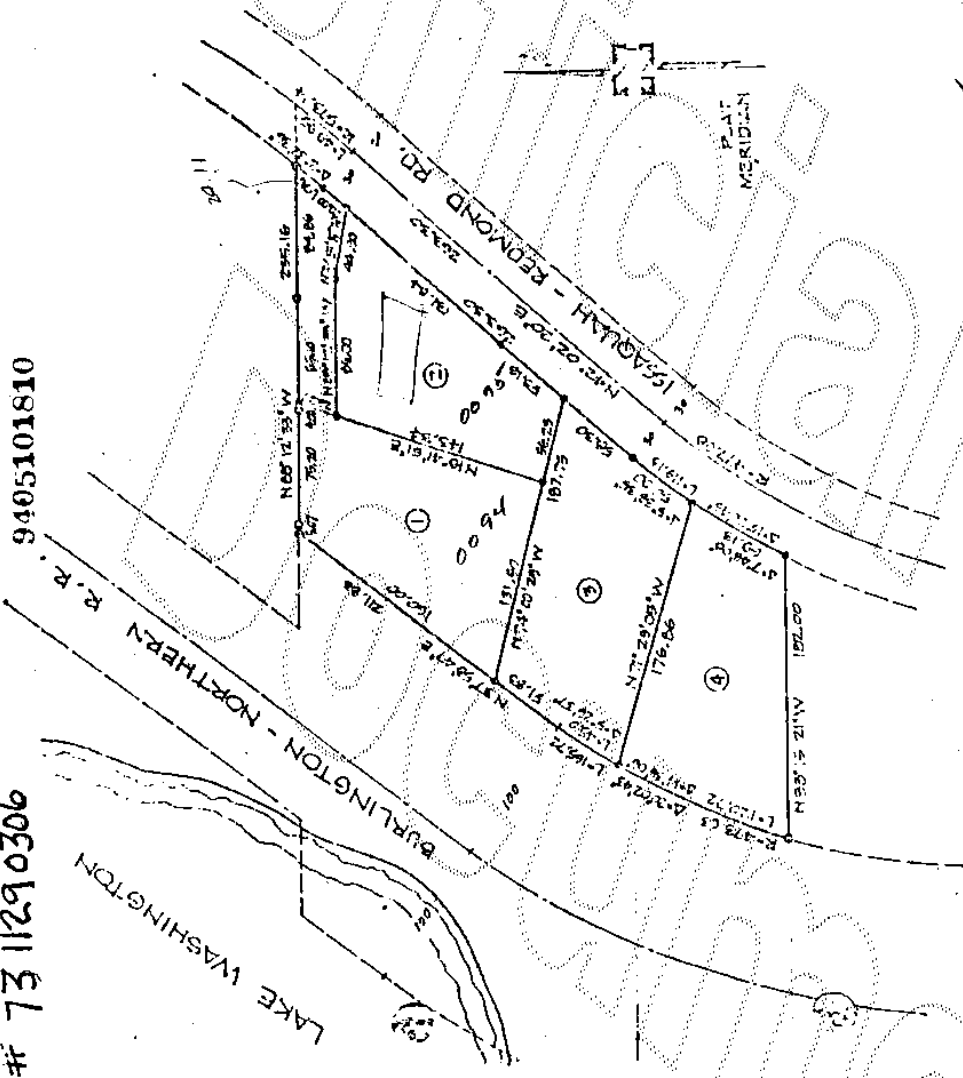
W. Williams
1979

9405101810

73 11290306

773058

KROLL S47-E



HART, WILLIAMS & ROTH, INC. PROPERTY SURVEY

Department of Assessments
E. 10th May 10 1994
Scott Noble Assessor
P. M. Mather Deputy Assessor

SE1/4 06-24-06
892010-0094/0095/0096/0097
TELL

REVISED DATE REC'D - Exhibit 29
SSDP2016-00415
002539

*Revised Drawing
4/5/79*

March 23, 1979

Order No. 704758

DESCRIPTION:

Parcel 1:

That portion of Tract "A" of View Point Park, as per plat recorded in Volume 44 of Plats, on pages 35 and 36, records of King County, Washington, more particularly described as follows:

Beginning at the Northwest corner of said Tract "A";
thence South $37^{\circ}58'47''$ West along the West line thereof a distance of 160.00 feet;
thence South $74^{\circ}00'29''$ East a distance of 131.50 feet;
thence North $19^{\circ}41'51''$ East a distance of 143.34 feet;
thence South $88^{\circ}12'33''$ East a distance of 86.00 feet;
thence South $76^{\circ}39'15''$ East a distance of 44.90 feet to a point on the East line of said Tract "A";
thence North $42^{\circ}02'20''$ East along said East line a distance of 20.00 feet to a point of curve, said curve having a radius of 543.14 feet;
thence Northeasterly along said curve concave to the Northwest, through a central angle of $2^{\circ}32'36''$, a distance of 24.11 feet to the Northeast corner of said Tract "A";
thence North $88^{\circ}12'33''$ West along the North line thereof a distance of 235.16 feet to the Point of Beginning.

Parcel 2:

That portion of Tract "A" of View Point Park, as per plat recorded in Volume 44 of Plats, on pages 35 and 36, records of King County, Washington, more particularly described as follows:

Commencing at the Northeast corner of said Tract "A", which is also a point on a curve having a radius of 543.14 feet, a radial line through said point bears South $50^{\circ}30'16''$ East from the center;
thence Southwesterly along the East line of said Tract "A" and along said curve concave to the Northwest through a central angle of $2^{\circ}32'36''$ a distance of 24.11 feet;
thence South $42^{\circ}02'20''$ West along said East line a distance of 20.00 feet to the True Point of Beginning;
thence continuing along said East line South $42^{\circ}02'20''$ West a distance of 185.00 feet;
thence North $74^{\circ}00'29''$ West a distance of 56.25 feet;
thence North $19^{\circ}41'51''$ East a distance of 143.34 feet;
thence South $88^{\circ}12'33''$ East, a distance of 86.00 feet to a point which bears North $76^{\circ}39'15''$ West from the True Point of Beginning;
thence South $76^{\circ}39'15''$ East, a distance of 44.90 feet to the True Point of Beginning;
TOGETHER WITH easement for drainfield purposes as recorded under Recording No. 731011-0438; and
TOGETHER WITH easement for ingress and egress as recorded under Recording No. 731004-0433.

ALL Situate in the County of King, State of Washington.

9405101810

Exhibit 29
SSDP2016-00415
002540

VIEW POINT PARK

Section 6, Twp. 24N. R. 6 E. W. M.

May 1947

SHEET 1 OF 2 SHEETS

General Engineering Co., Inc.
Engineers

DESCRIPTION

All of Government Lot 4, Section 6, Township 24 North, Range 6 East, W.M. lying east of the east margin of the right of way of the Northern Pacific Railroad; except the 60 foot right of way of the Issaquah-Redmond Road and the East 43.20 thereof.

DEDICATION

KNOW ALL MEN BY THESE PRESENTS that we the undersigned Fritz D. Sutter, Helen L. Sutter, his wife, and J.A. Holmes, Mortgagee, owners in fee simple of the land hereby platted, hereby declare this plat and dedicate to the use of the public forever, all streets, avenues and alleys shown here on and the use thereof for all public purposes not inconsistent with the use thereof for public highway purposes; also all parks, easements or whatever public property or places there are shown on the plat for the purpose there on indicated; also, the right to make all necessary slopes for cuts or fills upon the lots, blocks, tracts or parcels of land shown on this plat in the original reasonable grading of all the streets, avenues, alleys and places shown here on.

IN WITNESS WHEREOF we have hereunto set our hands and seals this 13th day of June, A.D. 1947.

J. A. HOLMES

FRITZ D. Sutter

HELEN L. Sutter

RESTRICTIONS



No lot or portion of a lot in this plat, shall be divided and sold or resold, or ownership changed or transferred, whereby the ownership of any portion of this plat shall be less than six thousand (6000) square feet for R-1 residence use, with a minimum lot width of sixty (60) feet. All lots in this plat are restricted to R-1 residence use, except tracts A and B, which are restricted to R-3 residence use, governed by and subject to restrictions, rules and regulations of King County Zoning Resolution No. 6494 and subsequent changes thereto by official County resolution.

ACKNOWLEDGMENT

STATE OF WASHINGTON } ss.
COUNTY OF KING

This is to certify that on this 13th day of June, A.D. 1947 before me the undersigned, a Notary Public, personally appeared Fritz D. Sutter, Helen L. Sutter, his wife, and J.A. Holmes, Mortgagee, to me known to be the individuals who executed the above dedication, and each acknowledged the said instrument to be his free and voluntary act and deed for the uses and purposes therein mentioned.

IN WITNESS WHEREOF I have hereunto set my hand and affixed my official seal the day and year first above mentioned.

Notary Public in and for the
State of Washington
Residing in Seattle.
 

I hereby certify that the within plat of VIEW POINT PARK is duly approved by the KING COUNTY PLANNING COMMISSION this 23rd day of June, A.D. 1947.

Chairman

Secretary

Executive Officer

I hereby certify that the plat of View Point Park is based on an actual survey and subdivision of section 6, township 24 north, range 6 east, W.M. that the distances, courses and angles are shown there on correctly; that the monuments have been set and lot and block corners staked correctly on the ground, or earnest money has been posted to covenant thereof; that I have fully complied with the provisions of the statutes and platting regulations.

General Engineering Co., Inc.

Date 16 June 1947

By A. J. Oliver
Certificate No. 1789 Renewal No. 801

Examined and approved this 23rd day of June, A.D. 1947.

Examined and approved this 21st day of June, A.D. 1947.

Chairman of the Board of
County Commissioners.

SIGNATURES ILLEGIBLE DUE TO USE OF COLORED INKS

County Road Engineer.



Attest: _____
Clerk
Board of County Commissioners

3699936
Filed for the Record at the request of the King County Planning Commission this 27th day of June, A.D. 1947, at 23 minutes past 2 P.M. and recorded in Vol. 11 of Plats, Page 35 of King County.

Deputy County Auditor

002541

County Auditor.

VIEW POINT PARK

Section 6, Twp. 24 N. R 6 E. W.M.

May 1947

SHEET 2 OF 2 SHEETS

Scale: 1 in. = 100 feet.

General Engineering Co., Inc.
Engineers

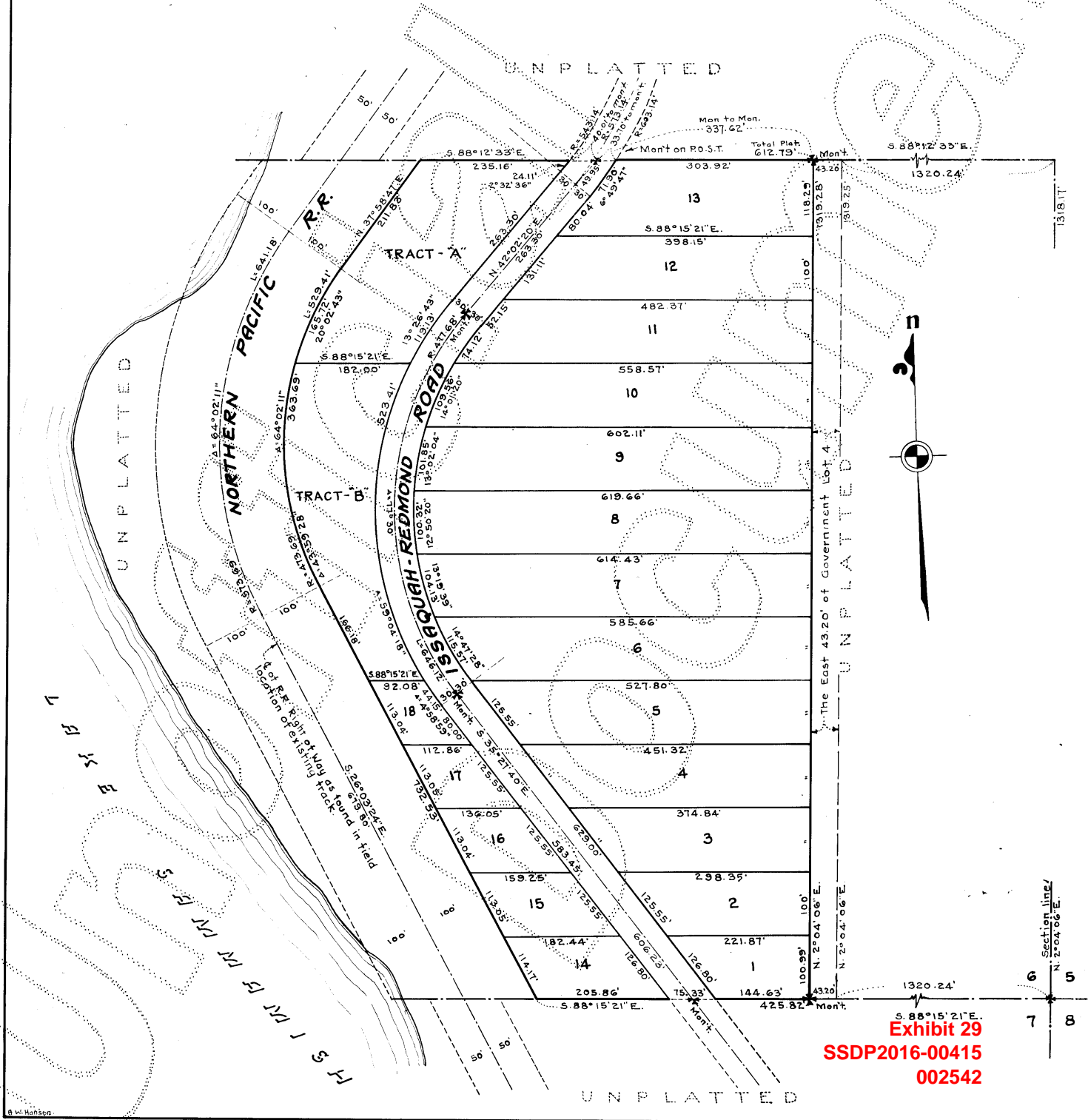


Exhibit 29
SSDP2016-00415
002542

OFFICERS

FRANK L. GRIMER, PRESIDENT
J. BOSS STEPHENSON, 3RD. PRESIDENT
ANDREW L. WOLD, SECRETARY
THEO. KINQUH, TREASURER
AL J. PETERS, DISTRICT TRUSTEE

ISSAQUAH KIWANIS CLUB

ON BEAUTIFUL LAKE SAMMAMISH

1925 REDMOND ROAD



ISSAQUAH, WASHINGTON



March 28, 1930

Honorable Board of County Commissioners
County-City Bldg.
Seattle, Washington.

Dear Sirs:

Inasmuch as a controversy has arisen in regards to the location of the new Issaquah-Redmond road and several resolutions have been presented to your honorable board concerning the location of same, we hereby present the following resolution:

"Be it resolved by the Issaquah Kiwanis Club that the new road between Issaquah and Redmond be located in such place that it will be permanent and acceptable to the State Highway Department for paving."

Very truly yours,

Issaquah Kiwanis Club

A. L. Stald.

Secy.

AIW:CH

Seattle, Wash.
809 5th Ave.

To the King County Commissioners
Seattle, Wn.

March

RECEIVED
MAR 19 1930
O. F. O'CONNOR,
SECRY NEXY OF KING
COUNTY COMMISSIONERS

Gentlemen:

I wish to file a protest against the proposed change in the county road on Lake Sammamish, near Monohon.

I own a small piece of land near this point and this change would greatly damage my property, as the County Engineers have staked off a new road directly across my land.

Very truly yours,

Isabel Keeling.



SB-52

TO THE HON. BOARD OF COUNTY COMMISSIONERS

Whereas, we the members of the Issaquah Valley Grange are very much interested in the locating of the new Issaquah-Redmond road on the east side of Lake Sammamish, particularly the 1½ mile stretch north of Monohon, and believing the Commissioners desirous of knowing the wishes of the people we are taking this means of giving our views.

Whereas, the building of the road on the survey along the Northern Pacific right of way would destroy the scenic beauty of the drive and would ruin much valuable property. According to our county engineer the building of the road there would be more costly there than the straightening and alignment of the present road.

And Whereas, the property owners are willing to give additional right of way required to do this. Therefore, be it resolved that we unanimously go on record this 10th day of March urging our Commissioners to give careful consideration of using present right of way.

Harold Tyne
Master

Ed. Washburn
Secretary

Signed by Committee

F. E. Kautbner

E. A. Hill

Wendell Thompson

E. J. Olson

A. J. Johnson

Exhibit 29
SSDP2016-00415
002545

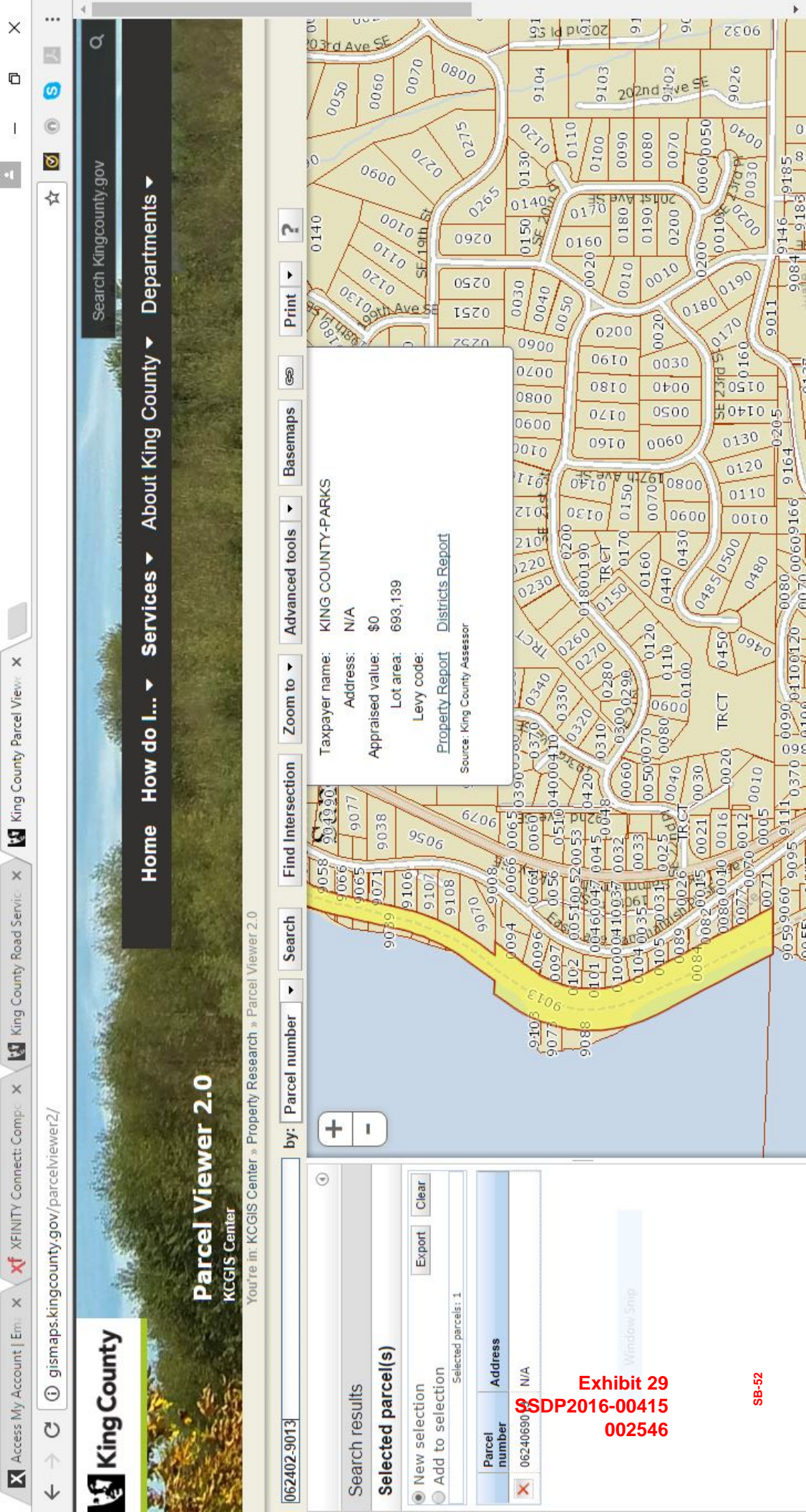
adopted

3 - 1030
SB-52

RECEIVED

MAR 17 1930

OFFICE OF THE
COUNTY COMMISSIONERS



Parcel Viewer 2.0

KGIS Center

You're in: KGIS Center » Property Research » Parcel Viewer 2.0

062402-9013

by: Parcel number Search Find Intersection Zoom to Advanced tools Basemaps Print ?

Search results

Selected parcel(s)

New selection Add to selection

Export Clear

Selected parcels: 1

Parcel number	Address
06240690	N/A

Taxpayer name: KING COUNTY-PARKS

Address: N/A

Appraised value: \$0

Lot area: 693,139

Levy code:

[Property Report](#) [Districts Report](#)

Source: King County Assessor

Exhibit 29
6SDP2016-00415
002546

SB-52